

#### IV. ALTERNATIVES

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## **IV. Alternatives**

Five Alternatives to the Proposed Action have been developed and compared with the Proposed Action. The Alternatives have been analyzed with respect to the potential impacts, including areas of land disturbance, traffic generation, air pollution, water use, sewage flow and tax generation. The comparison is shown on Table IV-1, which is located at the end of this Section. The five Alternatives are described as follows:

### **A. No Build**

The No Build Alternative A is required by SEQR to be described in the DEIS. For this site, the No Build Alternative assumes the site would continue to remain in its current state with none of the beneficial impacts of the proposed development. The property's current uses: two residences, Zino's nursery, King Gates & Fences and the abandoned motel, would be maintained. An analysis of the impacts of the No Build Alternative is as follows:

#### **1. Land Use and Zoning**

Maintaining the current uses would have no specific short term impact on land use and zoning. The abandoned motel site would remain in its current abandoned state. The existing deteriorated conditions would remain a blighting influence on the community.

#### **2. Visual Character**

If the No Build Alternative was implemented, the existing deteriorated state of the former motel would remain and would continue to adversely affect the safety and visual character of the neighborhood.

#### **3. Soils, Topography and Steep Slopes**

The No Build Alternative assumes that the project site would remain in its current state, resulting in no impact to the existing land topography. The existing potentially hazardous soil conditions identified on the site would not be cleaned up and the potential for further dumping and deterioration remains.

#### **4. Hazardous Materials**

The existing hazardous materials noted in the Phase I and II Environmental Site Assessments would remain unchanged on the site. The several ASTs, USTs, subsurface sanitary disposal systems, ACMs, lead based paint and contaminated soils would not be removed/remediated.

#### **5. Flora and Fauna**

The No Build Alternative would have no short term change on the Project Site vegetation. Long term impacts would result in the continued maturation of vegetation and plant overgrowth on the existing motel site. The non maintained and littered landscape character would remain.

6. Wetlands, Groundwater and Surface Water Resources  
The No Build Alternative would have no impact to existing wetlands, groundwater or surface water on or in the vicinity of the project site.
7. Stormwater Management  
The No Build Alternative would have no change to the surface water resources of the project site and would have no impact on stormwater drainage. However, without the Proposed Action, the reduction in stormwater peak discharge rates and the improvement to the existing head wall on Old Crompond Road would not be realized.
8. Utilities  
The No Build Alternative would have no impact on water demand, sewage flow or the telecommunications systems. Utility usage would remain unchanged. However, the extension of the existing sanitary sewer and the natural gas mains would not be extended along Old Crompond Road and the benefit of public sewer and gas to the residents of Old Crompond Road would not be realized.
9. Use and Conservation of Energy – Green Technology  
The No Build Alternative would have no impact to current use of energy on or in the vicinity of the Project Site. The energy usage would remain unchanged.
10. Solid Waste  
The No Build Alternative would have no impact to existing generation of solid waste on or in the vicinity of the Project Site. The generation of solid waste would remain unchanged.
11. Traffic and Transportation  
The No Build Alternative would have no impact on traffic conditions on or in the vicinity of the Project Site. Traffic patterns and volumes would remain unchanged.
12. Parking  
The No Build Alternative would have no impact to existing parking on or in the vicinity of the Project Site.
13. Air Quality  
The No Build Alternative would have no impact on air quality on or in the vicinity of the Project Site. The present air quality would remain unchanged.
14. Noise  
The No Build Alternative would have no impact on noise quality on or in the vicinity of the Project Site.
15. Building Demolition and Construction

The No Build Alternative would result in no change to the Project Site. The abandoned motel site would not be demolished and redeveloped. The existing deteriorated conditions of the abandoned motel site would remain a blighting influence on the community.

16. Community Facilities and Services

The No Build Alternative would have no impact on police protection, fire protection, emergency services or solid waste. Due to the undeveloped and abandoned condition of the former motel site, police patrols of the site would need to continue to be provided.

17. Fiscal and Socio-Economic Impacts

Implementation of the No Build Alternative would leave the Project Site in its current underutilized state. The lack of new development on the site would preclude any additional meaningful economic contribution that the Project would make in terms of new jobs and tax revenues. The sales, property and income taxes, as well as employment opportunities associated with the Proposed Action would not be realized under this No Build scenario. The existing condition of the former motel site would continue to deteriorate under this alternative.

18. Cultural Resources (Historical and Archeological)

Under the No Build Alternative, the Project Site would remain in its current state. While this would have no impact on the historical and archeological resources, the unattractive abandoned site would remain in the neighborhood.

Conclusion:

In summary, the No Build Alternative would not impact or increase land disturbance, traffic generation, air pollution, water use, sewage flow and tax generation. Neither would the No Build Alternative clean up the hazardous conditions or improve the deterioration of the site. In addition, the No Build Alternative would not realize the beneficial impacts that are expected to occur with the Proposed Action such as: improved traffic flow, removal of hazardous materials, demolition of abandoned buildings and infrastructure, extension of the existing sewer and gas services and generation of tax revenue.

**B. The Proposed Action With the Building Sited Further West on the Property, Away From the Taconic State Parkway**

Alternative B sites the building further west on the property. The building would be located in the area of steep slopes requiring fill beneath the building. The Applicant's geotechnical engineer indicates that the onsite soil can be used as general fill beneath the parking areas; however, it is not acceptable for use as structural fill beneath the building. Therefore, all of the fill required beneath the building must be imported structural fill. By comparison, the Proposed Action locates the building further east, where very little structural fill will be required.

This alternative would require significantly more construction traffic necessary to import the structural fill material as well as exporting excess excavated site material.

While this alternative places the building further from the Taconic Parkway it would still remain visible to parkway drivers except as screened by proposed landscaping. In addition, under this alternative, the building would be nearer the site's residential neighbors on Old Crompond Road.

**C. Alternative Site Layouts that Avoid Direct Impacts to Wetland Buffer Areas**

Alternative C provides an alternative site layout that is intended to minimize impacts to the wetland buffer areas. In this alternative, the building is shifted south to avoid the buffer to Wetland B. The loading area is shifted to the northwest corner of the building also to avoid impact to the Wetland B buffer. The parking layout is arranged to minimize impact to the Wetland A buffer. The result is that insufficient parking is provided on grade and a parking deck would be necessary to provide the remaining parking spaces. Costco has indicated that it could not go forward with a project where required parking would be placed on a deck. In addition, the Applicant has indicated that the added cost for deck parking would render the project too expensive to build and economically infeasible.

**D. Commercial Center Employing a Group of Buildings Per Zoning, Including a Village-Like Development**

Alternative D provides a village-like development consisting of several buildings arranged throughout the site. The development is a mixed use which includes a bank, restaurant and retail. By comparison the building area for this alternative is 92,465 square feet as compared to the Proposed Action which is 151,092 square feet. The land disturbance for both developments is similar and therefore Alternative D has less efficient use of the property. In addition, the required traffic improvements would be identical to the Proposed Action; the development costs would remain similar but the lower building square footage would reduce project revenue. The Applicant has indicated that under this alternative, the project would not be economically feasible.

**E. Hotel or Motel Development**

Alternative E provides a mixed use development in which a hotel is the central component. The development includes a 3-story, 136-room hotel supported by three standalone retail stores, a bank and two restaurants. By comparison the building area for this alternative is 133,209 square feet as compared to the Proposed Action which is 151,092 square feet. The land disturbance for both developments is similar and therefore Alternative E has less efficient use of the property. In addition, the required traffic improvements would be identical to the Proposed Action; the development costs would remain similar but the lower

building square footage would reduce project revenue. The Applicant has indicated that under this alternative, the project would not be economically feasible.

In addition and based on generalized knowledge of the project area, it is the opinion of the Applicant's professional team that the Yorktown Heights area has insufficient demand from either the business or tourism industries to support hotel/motel space at this time. This is further supported by the history of the project site in which the former motel did not remain successfully viable.

Really? And where is the nearest name brand hotel? It's not tourism, it's people coming to visit residents?!

Table IV.1 Alternatives Impact Comparative Analysis													
Alternative	Disturbance	Impervious	Traffic (1)						Air Quality	Water Demand gal./day	Sewage Flow gal./day	Annual Property Tax Generation	
			AM		PM		SAT						
			ENTRY	EXIT	ENTRY	EXIT	ENTRY	EXIT					
Proposed Action	14.0	10.91	45	19	240	240	240	388	388	388	5,500	5,000	\$158,622
Alt A No Build (4)	10.15	2.90	11	13	21	21	21	25	25	25	751	788	\$110,490
Alt B Building West	14.14	10.67	45	19	240	240	240	388	388	388	5,500	5,000	\$158,622
Alt C Wetland Avoidance	12.75	9.32	45	19	240	240	240	388	388	388	5,500	5,000	\$161,559
Alt D Village- Like Center	13.92	9.66	99	73	254	246	246	329	329	323	9,576	8,705	\$170,371
Alt E Hotel Mix	14.08	9.41	128	91	238	224	224	306	306	287	23,321	21,201	\$176,246

NOTES:

- (1) Trip generation numbers for alternatives represent new trips to the transportation system.
- (2) CO (carbon monoxide) emissions below Air Quality Standards.
- (3) GHG (green house gas) and Criteria Pollutants below Major Source Thresholds.
- (4) No Build condition assumes the motel remains vacant.



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Exhibit IV-A  
 No Build



COSTCO WHOLESALE  
 Town of Yorktown, New York





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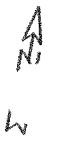


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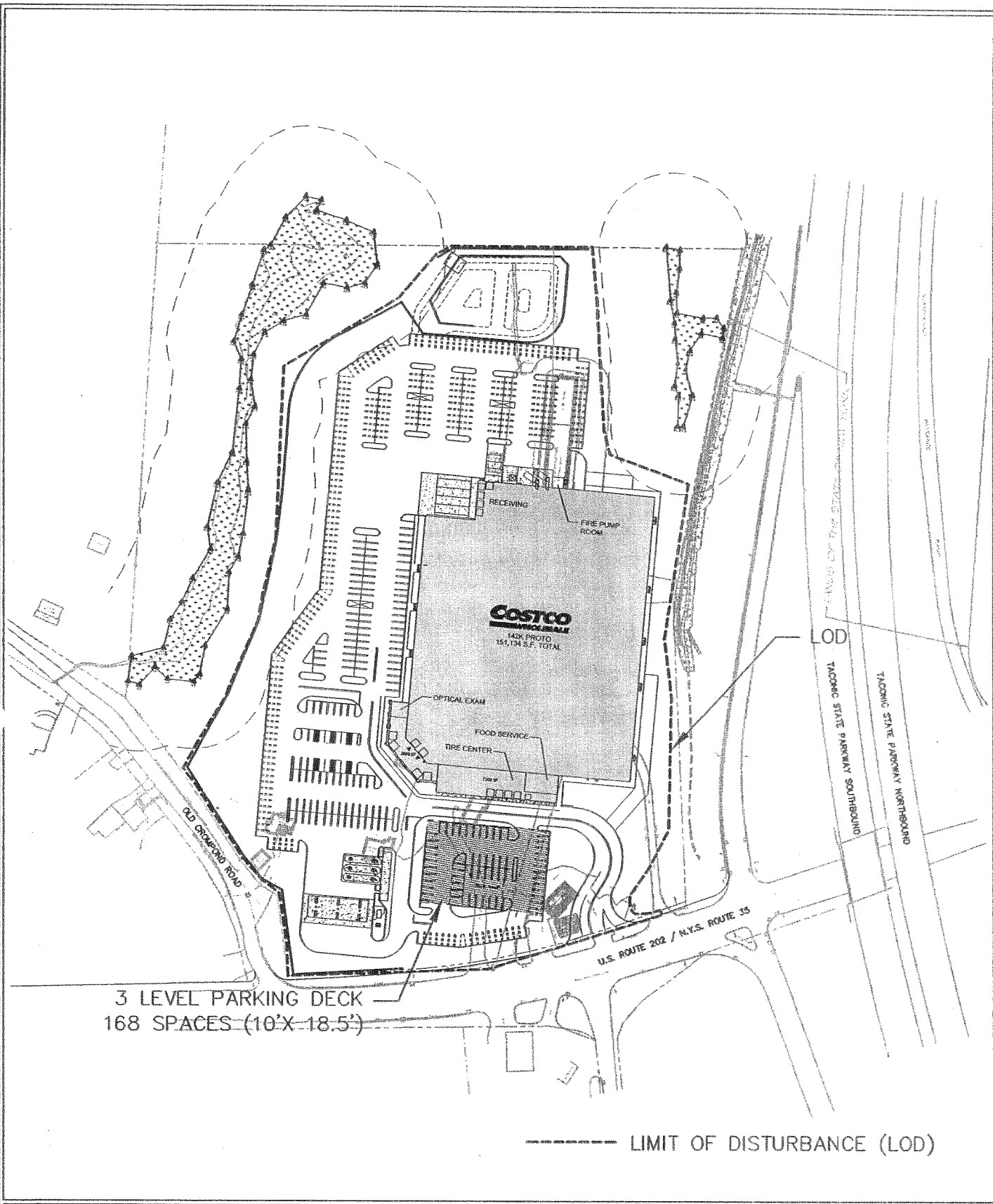
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Exhibit IV-B  
 Building Further From Taconic Parkway

COSTCO WHOLESALE  
 Town of Yorktown, New York



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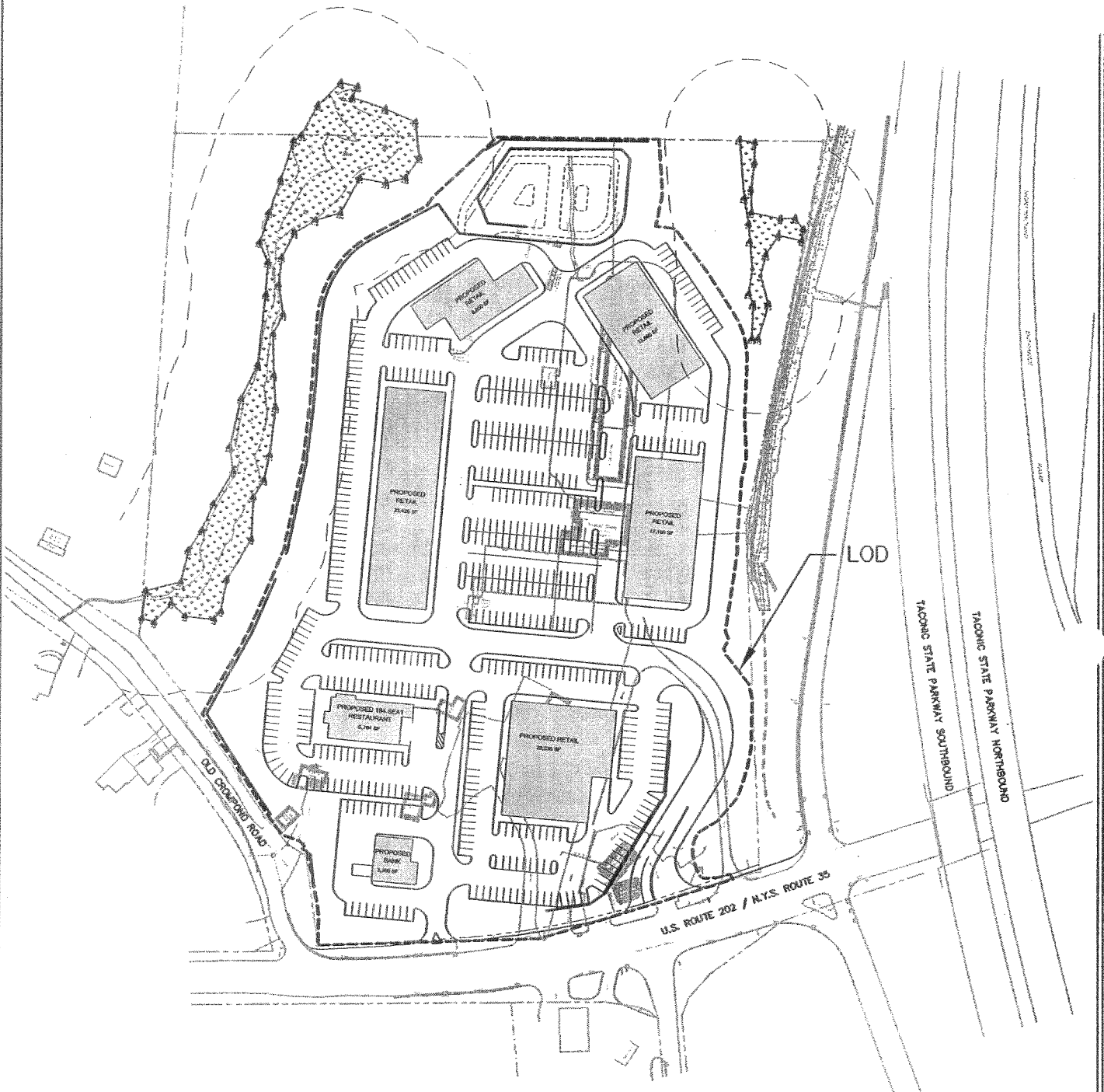
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### Exhibit IV-C Avoid Impact to Wetland Buffer

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--- LIMIT OF DISTURBANCE (LOD)



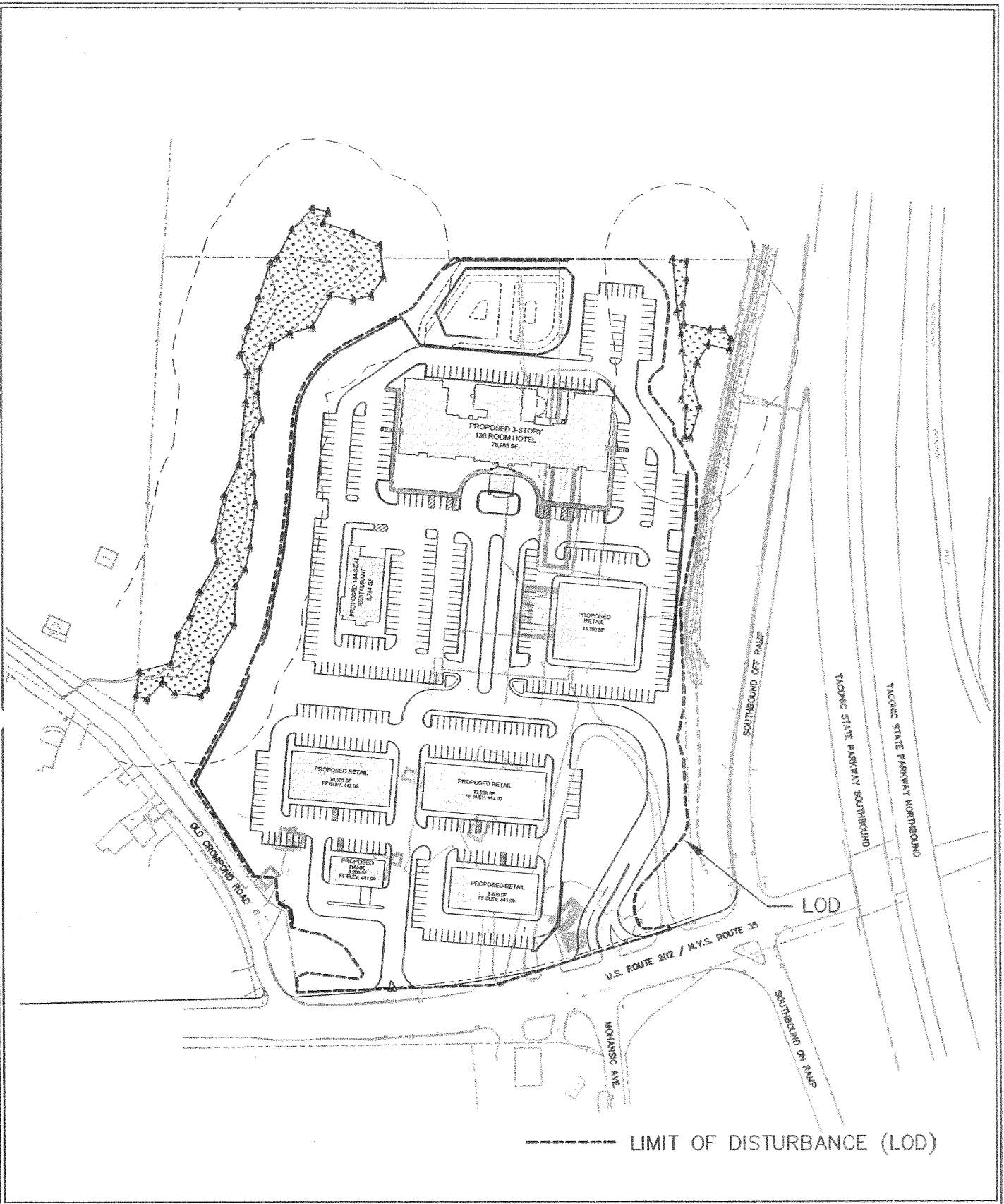
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Exhibit IV-D  
Commercial Center  
(Village-Like Development)



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### Exhibit IV-E Hotel/Motel Development

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