

LOCATION MAPS N.T.S.

	Sheet List Table	
Sheet No.	Sheet Title	Shee
G-0	COVER SHEET	1
G-1	LEGEND, ABBREVIATIONS AND GENERAL NOTES	2
C-1	OVERALL LOCATION MAP - STRAWBERRY ROAD AND HANOVER STREET AREA	3
C-2	STRAWBERRY ROAD - STA. SR 0+00 TO STA. SR 8+00	4
C-3	STRAWBERRY ROAD - STA. SR 8+00 TO STA. SR 13+62	5
C-4	HANOVER STREET - STA. HS 0+00 TO STA. HS 8+42	6
C-5	HANOVER STREET - STA. HS 8+42 TO STA. HS 16+48	7
C-6	HANOVER STREET - STA. HS 16+48 TO STA. HS 28+12	8
C-7	HANOVER STREET - STA. HS 28+12 TO STA. HS 39+58	9
C-8	HANOVER STREET - STA. HS 39+58 TO STA. HS 51+48	10
C-9	HANOVER STREET - STA. HS 51+48 TO STA. HS 59+98	11
D-1	CONSTRUCTION DETAILS	12
D-2	CONSTRUCTION DETAILS	13
D-3	SOIL EROSION AND SEDIMENT CONTROL DETAILS	14
T-1	TRAFFIC MAINTENANCE AND PROTECTION	15
T-2	TRAFFIC MAINTENANCE AND PROTECTION	16
T-3	TRAFFIC MAINTENANCE AND PROTECTION	17
T-4	TRAFFIC MAINTENANCE AND PROTECTION	18

Total 18 18 18 18 of 18 of 18 18 of 18 of 18 of of 18 18 of 18 18 of 18 of 18 of 18 of 18 of 18 of

TOWN OF YORKTOWN, NEW YORK

CLEANING AND CEMENT MORTAR LINING OF WATER MAIN ALONG STRAWBERRY ROAD AND HANOVER STREET

WATER SUPERINTENDENT **KEN RUNDLE**

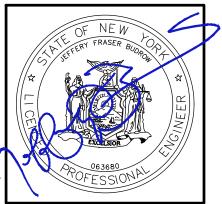


Weston & Sampson Engineers, PE, LS, LA, PC 100 South Bedford Road, Suite 340 Mount Kisco, NY 10549 800.SAMPSON

914.200.1077 www.westonandsampson.com

ASSISTANT SUPERINTENDENT PAUL VASILLO

JUNE 2022



BID SET #

SHEET 1 OF 18

LEGE	ND	
DESCRIPTION	EXISTING	PROPOSED
SANITARY SEWER	SS	
WATER MAIN	w	
WATER MAIN (ABANDONED)		××
GRINDER PUMP	O GP	GP
SANITARY SEWER MANHOLE	©	SMH DMH
STORM DRAIN MANHOLE		
		6" HOUSE
HOUSE CONNECTION		CONN (TYP)
AIR RELEASE VALVE MANHOLE	۵	• ARMH
FORCE MAIN CLEANOUT MANHOLE	©	● FMMH
CLEANOUT	0	• CO
CATCH BASIN		CB
CATCH BASIN (CURB INLET)		
HYDRANT	,	, Ç
TEMPORARY HYDRANT		H
GATE VALVE	м	M
CHECK VALVE		F
CURB STOP	*5	*
BUTTERFLY VALVE		
BALL VALVE	<u>ک</u>	►
REDUCER	۵ ۲	•
CAP OR PLUG	E CV	C
GAS GATE VALVE	X	
UTILITY POLE		
GUY POLE	<u>گ</u>	
	<u></u>	
EDGE OF PAVEMENT		
UNPAVED ROAD		
CURB		
SIDEWALK)	در
RAILROAD		
STONE WALL RETAINING WALL		
FENCE	RET_WALL	RET WALL
INDIVIDUAL DECIDUOUS TREE	(\cdot)	\odot
INDIVIDUAL EVERGREEN TREE		
SURVEY MARKER		
PROPERTY LINE		
EASEMENT LINE		
LIMIT OF WORK		· ·
APPROX. LIMIT OF REFUSE		
SPOT ELEVATIONS	× 100.2	×101.5
CONTOUR LINES	56	
DEPRESSION CONTOUR LINES		
UNDERGROUND GAS MAIN		
UNDERGROUND FIBER OPTIC BUNDLE		
HOUSE NUMBER	#35	
FLOOR ELEVATION	FL=56.7	
SILL ELEVATION	S=56.7	
WETLAND	<u>}}</u>	38888888
WETLAND FLAGS	•1	▲
RIP RAP		•
GUARD POST		
BOLLARD	ОВ	В
SIGN		
BENCH MARK	PT	
PERCOLATION TEST	PT TP	● PT-1
TEST PIT	€1	
BORING	⊕B-1	● B-3
PROBE		• P-1
GROUNDWATER MONITORING WELL	+ WSE-1	1 -1 -1
AUGER		⊕ A−1
HAY BALES		
ROCK OUTCROP		

ABBREVIATIONS

WCDOH

AC ACCMP	
ACCHE	ASBESTOS CEMENT PIPE
ACCIMIE	ASPHALT COATED CORRUGATED METAL PIPE
ARBE	AS REQUIRED BY ENGINEER
ARV	AIR RELEASE VALVE
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
BC	BITUMINOUS CONCRETE
BIT	BITUMINOUS
BLDG	BUILDING
BM	BENCH MARK
BO	BLOW OFF
BV	BUTTERFLY VALVE
CATV	CABLE TELEVISION
CB	CATCH BASIN
CC	CONCRETE CURB
CI	CAST IRON
С.	CENTERLINE
CL	CEMENT LINED
CMP	CORRUGATED METAL PIPE
CONC	CONCRETE
CU FT	CUBIC FEET
CY	CUBIC YARD
D	STORM DRAIN, DEPTH FROM RIM TO INVERT
DI	DROP INLET, DUCTILE IRON
DIA DMH	DIAMETER DRAIN MANHOLE
DWG	DRAWNG
E	EAST, ELECTRIC
ĒA	EACH
EF	EACH FACE
ELEV	ELEVATION
EOP	EDGE OF PAVEMENT
EW	EACH WAY
EXIST	EXISTING
FLG	FLANGE
FT	FEET, FOOT
G	NATURAL GAS
GALV	GALVANIZED
GC	GRANITE CURB
GR	GRANITE
HC	HOUSE CONNECTION
HORIZ	HORIZONTAL
HP	HIGH PRESSURE
HYD	FIRE HYDRANT
I INV	INVERT INVERT
ID	INSIDE DIAMETER
IP	IRON PIPE
LB	POUND
LF	LINEAR FEET
LS	LUMP SUM
MAX	MAXIMUM
MB	MAIL BOX
MECH	MECHANICAL
MH	MANHOLE
MIN MISC	MINIMUM MISCELLANEOUS
MJ	MISCELLANEOUS MECHANICAL JOINT
N	NORTH
NE	NORTH EAST
NW	NORTH WEST
NF	NOT FOUND
NO OR #	NUMBER
NYSDOT	
	NEW YORK STATE DEPT. OF TRANSPORTATION
NYSDEC	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION
OD	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER
OD PCCP	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE
OD PCCP PE	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE
OD PCCP PE Æ	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE
OD PCCP PE FL PL	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE
OD PCCP PE PL PVC	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE
OD PCCP PE PL PVC PVMT	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT
OD PCCP PE EL PL PVC PVMT RCP	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE
OD PCCP PE PL PVC PVMT RCP ROW	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY
OD PCCP PE EL PL PVC PVMT RCP	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY
OD PCCP PE PL PVC PVMT RCP ROW RQD	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS)
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP UD	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL UNDER DRAIN
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP UD UP	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL UNDER DRAIN UTILITY POLE
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP UD UP VC	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SHEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL UNDER DRAIN UTILITY POLE VITRIFIED CLAY
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP UD UP VC VERT	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL UNDER DRAIN UTILITY POLE VITRIFIED CLAY VERTICAL
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP UD UP VC VERT W	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEVER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL UNDER DRAIN UTILITY POLE VITRIFIED CLAY VERTICAL WATER, WEST
OD PCCP PE PL PVC PVMT RCP ROW RQD S SE SECT SF SHT SPEC SQ FT SS STA STL SW T TBM THK TYP UD UP VC VERT	NEW YORK STATE DEPT. OF TRANSPORTATION NEW YORK STATE DEPT. OF ENVIRONMENTAL CONSERVATION OUTSIDE DIAMETER PRESTRESSED CONCRETE CYLINDER PIPE PLAIN END, POLYETHYLENE PROPERTY LINE PLATE POLYVINYL CHLORIDE PAVEMENT REINFORCED CONCRETE PIPE RIGHT-OF-WAY ROCK QUALITY SEWER, SOUTH SOUTH EAST SECTION SQUARE FEET SHEET SPECIFICATIONS SQUARE FEET SEWER SERVICE, STAINLESS STEEL STATION STEEL SIDEWALK, SOUTH WEST HYDROSTATIC THRUST, TELEPHONE TEMPORARY BENCH MARK THICK (NESS) TYPICAL UNDER DRAIN UTILITY POLE VITRIFIED CLAY VERTICAL

NOTE: ITEMS SHOWN IN THE LEGEND MAY NOT BE PRESENT IN THESE PLANS

All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are NOT intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC for the specific purpose intended will be at the sole risk of those individuals and without liability or legal exposure to Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants and those individuals shall indemnify and hold harmless Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC to further compensation rates at the time of such verification or adaptation from those individuals. All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

It is a violation of New York state education law article 145, enacted 07-31-73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a icensed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.

WESTCHESTER COUNTY DEPARTMENT OF HEALTH

CONSTRUCTION NOTES

IPE	

- 1-800-962-7962 AT LEAST 48 HOURS, SATURDAYS, SUNDAYS, AND HOLIDAYS EXCLUDED, PRIOR TO EXCAVATING AT ANY LOCATION.
 - 2. LOCATIONS OF EXISTING PIPES, CONDUITS, UTILITIES, FOUNDATIONS AND OTHER UNDERGROUND OBJECTS ARE NOT WARRANTED TO BE CORRECT AND THE CONTRACTOR SHALL HAVE NO CLAIM ON THAT ACCOUNT SHOULD THEY BE OTHER THAN SHOWN.

1. THE CONTRACTOR SHALL CALL DIG SAFELY NY AT

- 3. TEST PITS TO LOCATE EXISTING UTILITIES MAY BE REQUIRED BY THE ENGINEER TO DETERMINE WHETHER TO RAISE OR LOWER THE PROPOSED WATER MAIN TO CLEAR EXISTING UTILITIES OR VERIFY EXISTING UTILITY LOCATION, SIZE AND TYPE.
- 4. STONE WALLS, FENCES, MAIL BOXES, SIGNS, CURBS, LIGHT POLES, ETC. SHALL BE REMOVED AS NECESSARY TO PERFORM THE WORK AND REPLACED TO A CONDITION AT LEAST EQUAL TO THAT BEFORE CONSTRUCTION BEGAN. UNLESS OTHERWISE INDICATED, ALL SUCH WORK SHALL BE INCIDENTAL TO CONSTRUCTION OF THE PROJECT.
- 5. ALL PAVEMENT DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DRAWINGS.
- 6. ALL AREAS DISTURBED BY THE CONTRACTOR BEYOND PAYMENT LIMITS SHALL BE RESTORED AT NO ADDITIONAL COST TO THE OWNER.
- UNLESS OTHERWISE INDICATED, CONCRETE USED FOR PIPE ANCHOR BLOCKS, BACKING, PIPE CRADLES, ARCHES, AND FILL SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI AT 28 DAYS.
- 8. APPROVED JOINT RESTRAINT METHODS SHALL BE PROVIDED FOR WATER MAINS WHERE ANY BENDS, TEES, PLUGS, OR WYES ARE INSTALLED. CONCRETE THRUST BLOCKS. ANCHOR BLOCKS AND TIE RODS MAY BE USED FOR 6-INCH AND 8-INCH PIPE WHERE JOINT RESTRAINT IS NOT FEASIBLE. FOR THRUST BLOCK DETAILS AND MINIMUM BLOCK BEARING AREAS, SEE DETAILS AND SPECIFICATIONS.
- 9. THE CONTRACTOR SHALL NOT STORE ANY APPARATUS, MATERIALS, SUPPLIES, OR EQUIPMENT ON DRAINAGE STRUCTURES OR WITHIN 100 FEET OF WETLANDS.
- 10. NEW WATER MAINS AND SERVICES SHALL BE INSTALLED AT THE MINIMUM DEPTH FROM FINISH GRADE TO TOP OF PIPE AS SHOWN ON THE DRAWINGS. WHERE NECESSARY, NEW WATER MAINS SHALL BE INSTALLED AT A GREATER DEPTH TO CLEAR OBSTACLES SHOWN ON THE DRAWINGS AT NO ADDITIONAL COST TO THE OWNER. MINIMUM CLEARANCES TO UTILITIES, AS SHOWN ON THE DRAWINGS SHALL BE MAINTAINED.
- 11. EXISTING SERVICES SHALL NOT BE CONNECTED TO THE PROPOSED WATER MAIN UNTIL THAT MAIN HAS PASSED PRESSURE TEST AND DISINFECTION REQUIREMENTS.
- 12. ALL HYDRANTS REMOVED SHALL BE SALVAGED AND DELIVERED TO A LOCATION TO BE DETERMINED BY THE OWNER. SALVAGED ITEMS SHALL BE CLEANED AND RETURNED IN OPERATIONAL CONDITION TO THE SATISFACTION OF THE ENGINEER.
- 13. ANY HYDRANT WHICH IS NOT IN SERVICE SHALL BE COVERED WITH A SECURELY FASTENED BURLAP BAG.
- 14. VALVE BOXES ON MAINS TO BE ABANDONED SHALL BE REMOVED AND SALVAGED BY THE CONTRACTOR AND DELIVERED TO A LOCATION TO BE DETERMINED BY THE OWNER. ITEMS SHALL BE CLEANED AND RETURNED IN OPERATIONAL CONDITION TO THE SATISFACTION OF THE ENGINEER.
- 15. THE LOCATION OF PIPES, CAPS, REDUCERS, BENDS, AND OTHER FITTINGS AT POINTS OF CONNECTIONS TO EXISTING MAINS IS APPROXIMATE. CONTRACTOR SHALL DIG A TEST PIT AT EACH LOCATION TO DETERMINE THE DIAMETER AND MATERIAL OF THE EXISTING PIPE AND THE LOCATION OF THE TIE-IN POINT IN ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DRAWINGS.
- 16. THE CONTRACTOR SHALL NOTIFY THE OWNER/ENGINEER AT LEAST TWO (2) DAYS PRIOR TO THE START OF WORK IN A STATE ROADWAY.
- 17. ALL STREET EXCAVATIONS SHALL BE COMPLETELY CLOSED AT THE END OF EACH WORKING DAY BY BACKFILLING. COVERING WITH STEEL PLATES MAY BE ALLOWED IF APPROVED BY THE ENGINEER.
- 18. THE CONTRACTOR SHALL NOT OPEN OR CLOSE ANY VALVES WHICH HOLD WATER IN THE SYSTEM. THE OWNER WILL, ON 24 HOURS NOTICE FROM THE CONTRACTOR. OPEN AND/OR CLOSE ANY VALVES REQUIRED FOR DRAINING OR ADMITTING WATER TO THE VARIOUS SECTIONS OF THE WATER MAINS. THE CONTRACTOR IS RESPONSIBLE TO NOTIFY IN WRITING 24 HOURS IN ADVANCE, ANY OCCUPANT THAT WILL BE WITHOUT WATER DUE TO A SHUTDOWN.
- 19. SOME WATER SERVICE CONNECTIONS MAY NOT BE SHOWN ON THE DRAWINGS. THE OWNER WILL MARK THE LOCATION OF SUCH CONNECTIONS, PROVIDED THE CONTRACTOR GIVES THE OWNER AT LEAST 24 HOURS ADVANCE NOTICE.
- 20. WHENEVER PIPE LAYING IS NOT IN PROGRESS, CONTRACTOR SHALL KEEP A TEMPORARY PLUG IN PLACE TO AVOID ENTRY OF ANY FORM OF CONTAMINATION.

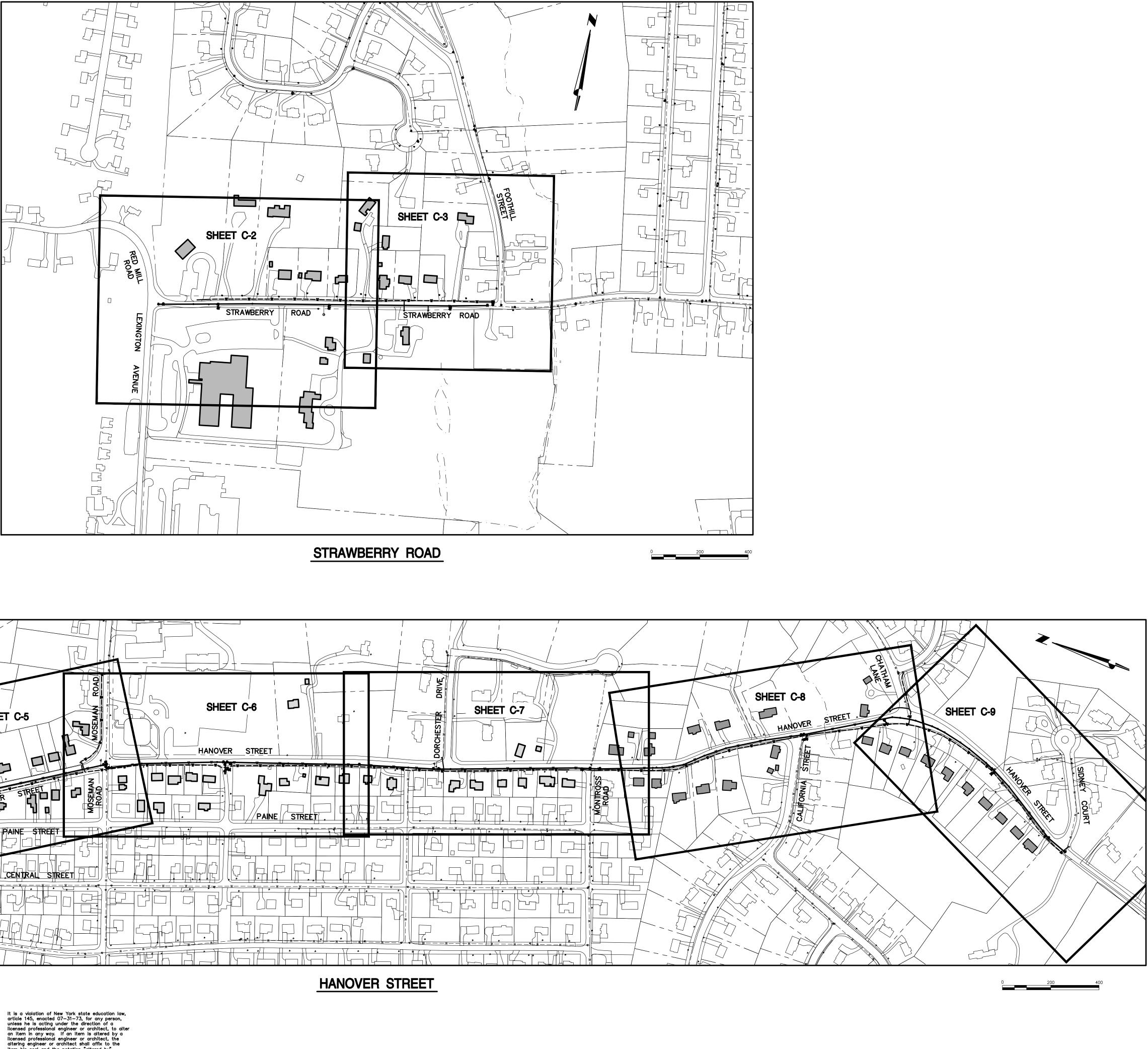
GENERAL NOTES

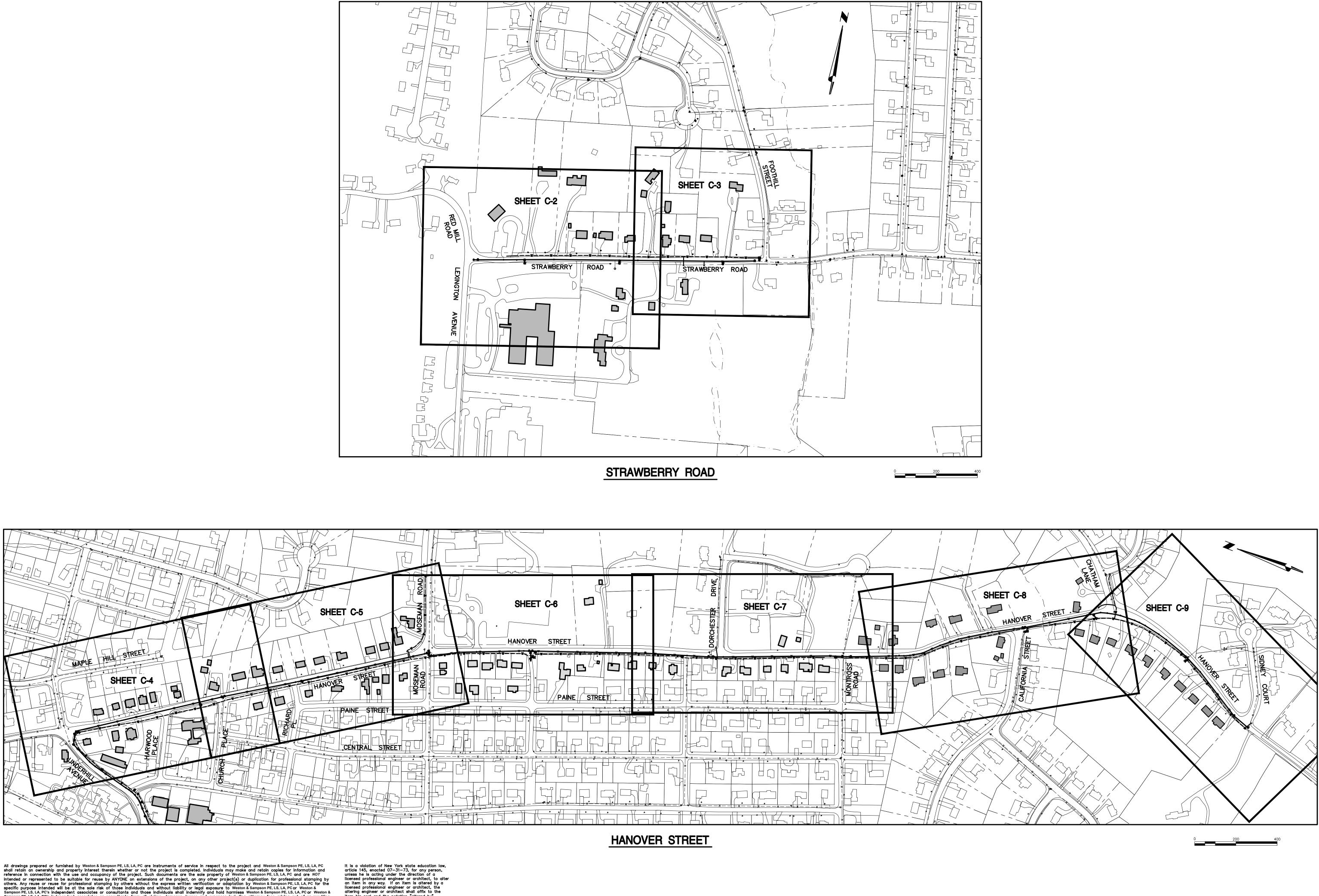
- 1. THE LOCATIONS. SIZES AND ELEVATIONS OF EXISTING UTILITIES DETERMINED ON INFORMATION COMPILED FROM DRAWINGS OF REC INFORMATION BY THE CONTRACTOR. THE CONTRACTOR SHALL COM CODE RULE 53 TO NOTIFY ALL UTILITIES PRIOR TO DIGGING.
- 2. THE APPROXIMATE LOCATION OF THE PROPOSED PIPELINE IS INDIC THE PLANS, BUT THE ACTUAL LOCATION WILL BE GOVERNED BY TH LOCATION OF THE UNDERGROUND UTILITIES OR OTHER CONTROLLING AS DETERMINED BY THE ENGINEER DURING CONSTRUCTION.
- 3. MINIMUM COVER ON ALL NEW PIPELINE SHALL BE 4.5' MEASUR FINISHED GROUND SURFACE EXCEPT AS OTHERWISE NOTED.
- 4. WHERE THE CLEARANCE BETWEEN THE WATERMAIN AND ANY EXISTIN OR SERVICE CONNECTIONS IS LESS THAN 1', A SAND CUSHION PROVIDED AT NO ADDITIONAL COST TO THE OWNER.
- 5. THE CONTRACTOR SHALL MAINTAIN CONTINUOUS, UNINTERRUPTED THROUGH THE EXISTING AND/OR TEMPORARY WATER LINES DUP COURSE OF THE CONSTRUCTION PERIOD.
- 6. ROADWAY DRAINAGE SHALL BE MAINTAINED THROUGHOUT THE PE CONSTRUCTION.
- 7. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT, DU NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EX RECONSTRUCTION WORK CANNOT ALWAYS BE ACCURATELY DETERMIN TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMEN BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INF AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY MODIFICATIONS IN CONSTRUCTION DETAILS AND WORK QUANTI CONTRACTOR SHALL VERIFY ALL EXISTING CONDITIONS AND DIME THE FIELD AND SHALL PERFORM THE WORK TO MEET FIELD CONDIT CONTRACTOR SHALL PERFORM ALL REPAIR WORK AS SHOWN ON IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS AND AS REC THE ENGINEER.
- 8. THE CONTRACTOR SHALL COMPLETE ALL WORK IN A MANNER SU ANY MATERIALS WHICH ARE TO REMAIN IN PLACE WILL NOT BE DAM THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO F PLACE, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLA MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE CONTRACTOR.
- 9. THE CONTRACTOR SHALL GIVE PARTICULAR ATTENTION AND PROTECT FROM DAMAGE ALL VEGETATION, INCLUDING TURF, TH SHRUBS NOT IN THE CONTRACT AREA.
- 10. WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL A TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE PRICE
- 11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GRADING AND RE ANY EARTH AREA DISTURBED BY HIS OPERATION WITHIN OR OUTSIE ACTUAL WORK AREA AND AS REQUIRED BY THE ENGINEER. ALL SHALL BE DONE IN A WORKMANLIKE MANNER AND TO THE SATISFA THE ENGINEER.
- 12. MAINTENANCE AND PROTECTION OF BOTH VEHICULAR AND P TRAFFIC SHALL BE PERFORMED IN ACCORDANCE WITH THE LATES OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MU PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION. AS WEL NEW YORK STATE DEPARTMENT OF TRANSPORTATION SUPPLEMENT MUTCD, A.O.B.E., WITH THE SPECS, AND AS SHOWN ON THE DRAWN
- 13. THE CONTRACTOR SHALL INSTALL TEMPORARY EROSION CONTROL D ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DR
- 14. THE CONTRACTOR SHALL PERFORM ALL NECESSARY SURVEY AND REQUIRED TO PERFORM THE WORK DESCRIBED HEREIN.
- 15. IF GROUNDWATER IS FOUND WITHIN 7' OF THE GROUND SURF HYDRANT DRAINS SHOULD BE PLUGGED. WHEN THE DRAINS ARE THE BARRELS MUST BE PUMPED DRY AFTER USE DURING FREEZING WHERE HYDRANT DRAINS ARE NOT PLUGGED, A GRAVEL POCKET WELL SHALL BE PROVIDED UNLESS THE NATURAL SOILS WILL ADEQUATE DRAINAGE. HYDRANT DRAINS SHALL NOT BE CONNECT LOCATED WITHIN 10' OF SANITARY SEWERS, STORM SEWERS, DRAINS.
- 16. THE SUPPLIER'S RECORDS INDICATE ADEQUATE PRESSURE AND CAR AVAILABLE FOR THE AREA IF THIS PROJECT RESULTS IN AN INC DFMAND.
- 17. ONLY DETAILS RELEVANT TO THE SPECIFIC PUBLIC WATER IMPROVEMENTS SHOWN ON THE PLANS ARE APPLICABLE.
- 18. THE DESIGN AND LOCATION OF SANITARY FACILITIES (WATER AN SYSTEMS) SHALL NOT BE CHANGED.
- 19. THIS SHEET IS INCOMPLETE AND INVALID UNLESS ACCOMPANIED BY THE OTHER SHEETS.
- 20. BASE MAPPING PROVIDED BY THE TOWN OF YORKTOWN.
- 21. UTILITY LAYOUTS BASED ON HISTORICAL G.I.S. UTILITY MAPPING PRO THE TOWN OF YORKTOWN.

MATERIALS AND SCHEDULE:

1. EXISTING FIRE HYDRANTS, GATE VALVES, AND VALVE BOXES ARE SALVAGED AND RETURNED TO THE YORKTOWN CONSOLIDATED WATER FACILITY LOCATED AT 1080 SPILLWAY ROAD, SHRUB OAK, NEW YORK

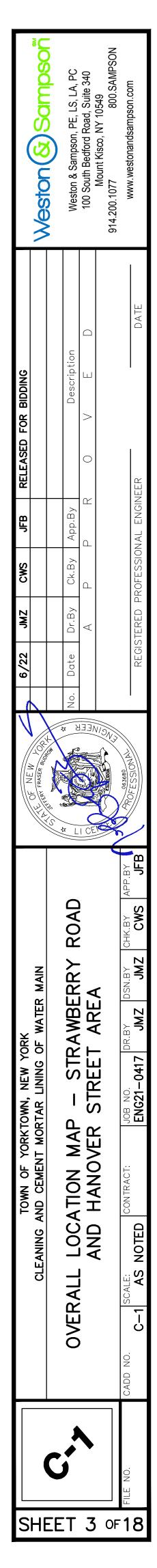
		WATER MAIN TESTING AND DISINFECTION NOTES		ື ຊ	Ş		SAMPSON	
SHALL BE XORD AND IPLY WITH	1.	WATER FOR TESTING AND FLUSHING SHALL BE OBTAINED FROM EXISTING WATER SYSTEM. ARRANGEMENTS SHALL BE MADE WITH THE WATER SUPERINTENDENT FOR PAYMENT OF WATER USED.		Č		LS, LA, PC , Suite 340	၈ ဝ	on.com
CATED ON E ACTUAL	2.	FLUSH MAINS AND SERVICES BEFORE TESTING. MINIMUM FLUSHING VELOCITY SHALL BE 2.5 FEET PER SECOND.		U.	}	on, PE, L rd Road,	ο, NY 10	ndsampso
FACTORS RED FROM	3.	THE SECTION UNDER TEST SHALL BE MAINTAINED FULL OF WATER FOR A PERIOD OF 24 HOURS PRIOR TO THE PRESSURE AND LEAKAGE TEST BEING APPLIED. FURNISH ALL WATER, EQUIPMENT, CONNECTIONS, PIPING, METERS, MEASURING DEVICES, PUMPS, AND TEMPORARY ENCLOSURES NECESSARY TO PERFORM THE REQUIRED			ソ	Weston & Sampson, PE, LS, 100 South Bedford Road, St	Aount Kiso 7	www.westonandsampson.com
		TESTS, SUBJECT TO THE REVIEW AND APPROVAL BY THE TOWN WATER SUPERINTENDENT AND ENGINEER. TESTING SHALL BE MADE ON SECTIONS OF WATER MAIN NOT EXCEEDING 2000 FEET IN LENGTH.		Mactor (2	Westor 100 Sc	۸ 200.107.	MM
SHALL BE	4.	TESTING SHALL MEET THE MINIMUM REQUIREMENTS OF AWWA C-600 SECTION 5, EXCEPT WHERE MORE RIGID REQUIREMENTS ARE ESTABLISHED BY THESE		3	> 		614	
SERVICE RING THE ERIOD OF		SPECIFICATIONS. BEFORE APPLYING TEST PRESSURE, ALL AIR SHALL BE EXPELLED FROM THE PIPE. AFTER THE PIPE HAS BEEN FILLED, IT SHALL BE SUBJECTED TO HYDROSTATIC PRESSURE OF 150 PSI OR 1.5 TIMES THE WORKING PRESSURE OF THE SYSTEM AS A MINIMUM, WHICHEVER IS GREATER, FOR A PERIOD OF TWO HOURS.						
IE TO THE TENT OF NED PRIOR	5.	PRELIMINARY SYSTEM TESTING MAY BE PERFORMED AT NORMAL OPERATING PRESSURE. THE TOWN MAY REQUEST TO VISUALLY INSPECT CONNECTIONS PRIOR TO ACCEPTANCE OF WORK. AFTER THE PRELIMINARY TEST IS SATISFACTORILY COMPLETED, THE ENGINEER SHALL BE GIVEN 24 HOURS NOTICE AND A FINAL TEST SHALL BE PERFORMED.						DATE
TS HAVE ORMATION REQUIRE	6.	LEAKAGE SHALL BE DETERMINED AT 30 MINUTE INTERVALS BY MEANS OF VOLUMETRIC MEASUREMENT OF THE WATER ADDED DURING THE TEST.		ING		escription E		
TIES. THE NSIONS IN IONS. THE HE PLANS QUIRED BY	7.	TEST PRESSURE SHALL BE BASED ON THE ELEVATION OF THE LOWEST POINT UNDER TEST. PRESSURE SHALL BE APPLIED BY A PUMP CONNECTED TO THE PIPE. THE PUMP, PIPE, CONNECTIONS, GAUGES, AND MEASURING DEVICES SHALL BE CALIBRATED TO THE SATISFACTION OF THE ENGINEER.		FOR BIDDING		< Des		
JCH THAT IAGED. IF REMAIN IN	8.	LEAKAGE SHALL BE DEFINED AS THE QUANTITY OF WATER SUPPLIED TO THE SECTION OF THE PIPE UNDER TEST NECESSARY TO MAINTAIN THE REQUIRED PRESSURE. SHOULD ANY TEST DISCLOSE LEAKAGE GREATER THAN THE ALLOWABLE, THE DEFECT SHALL BE LOCATED AND REPAIRED BY THE CONTRACTOR.		RELEASED		0		EER
CED IN A OF THE CARE TO REES AND	9.	ALL WATER MAINS AND APPURTENANCES SHALL BE DISINFECTED IN ACCORDANCE WITH AWWA C651-05 EXCEPT FOR SECTION 4.4.2 OR LATEST AWWA STANDARD DISINFECTING WATER MAINS, ITEM 5.1 DELETED, AND THE REQUIREMENTS OF N.Y.S. DEPARTMENT OF HEALTH, USING THE CONTINUOUS FEED METHOD. THE REQUIREMENTS OF N.Y.S. DEPARTMENT OF HEALTH SHALL GOVERN WHEN THERE IS A CONFLICT. USE 25 PPM INITIAL CHLORINE DOSE. DISINFECTANT SHALL REMAIN IN THE SYSTEM FOR A PERIOD OF 24 HOURS AFTER WHICH THE RESIDUAL SHALL BE AT				By App.By P R		SSIONAL ENGINEER
REMOVED REA AND BID.		LEAST 10 PPM. FOLLOWING DISINFECTION, ALL TREATED WATER SHALL BE THOROUGHLY FLUSHED FROM THE MAIN AND DECHLORINATED AS REQUIRED BEFORE BEING DISCHARGED. TABLET METHOD OF DISINFECTION IS NOT ACCEPTABLE.				P Ck.		PROFE
E-SEEDING DE OF THE THE WORK ACTION OF EDESTRIAN	10.	THE INTERIORS OF ALL APPURTENANCES AND SECTIONS OF WATER MAIN THAT CANNOT NORMALLY BE DISINFECTED SHALL BE SWABBED BY THE CONTRACTOR, TO THE SATISFACTION OF THE ENGINEER, WITH A CONCENTRATED CHLORINE SOLUTION CONTAINING NO LESS THAN 200 PPM OF FREE CHLORINE. THE CONTRACTOR SHALL ALSO DISINFECT ALL EXISTING WATER LINES AND APPURTENANCES WHICH WERE BROKEN, DAMAGED, CONTAMINATED, OR SUSPECTED OF BEING CONTAMINATED AS A RESULT OF WORK DONE WITH THIS PROJECT.	-	6/22 JMZ		Date Dr.By A		REGISTERED
T EDITION JTCD) AS L AS THE T TO THE GS. DEVICES IN	11.	FOR EVERY 1200' OF WATER MAIN, 2 CONSECUTIVE WATER SAMPLES SHALL BE COLLECTED AT LEAST 24 HOURS APART BY THE CONTRACTOR AND ANALYZED BY A NEW YORK STATE DEPARTMENT OF HEALTH APPROVED TESTING LABORATORY TO BE FREE OF COLIFORM BACTERIA AND OTHER BACTERIOLOGICAL CONTENT. THE WORK WILL NOT BE ACCEPTED UNTIL A REPORT IS SUBMITTED TO THE ENGINEER AND THE TOWN WATER DISTRICT SHOWING THAT A WATER SAMPLE IS SATISFACTORY AND THE SYSTEM IS READY FOR USE.		1 1 1 1 1 1 1 1 1	1, 1000 M	No.	NO NO	MAL
AWNGS. STAKEOUT	12.	CEMENT-MORTAR LINING AND ASSOCIATED WORK WILL BE PERFORMED IN ACCORDANCE WITH AWWA STANDARD C602-00 OR LATEST AWWA STANDARD.		OF NE N			E CONTRACTOR OF	OFESSIC
ACE THEN PLUGGED WEATHER. OR DRY PROVIDE ED TO OR OR STORM	13.	AFTER DISINFECTION OF THE CLEANED WATER MAINS, OR PORTIONS THEREOF, THE WATER MAINS SHALL BE FILLED WITH FRESH WATER AND TWO (2) BIOLOGICAL SAMPLES WILL BE COLLECTED AT AN INTERVAL OF 24 HOURS AT THE SAME LOCATION. IN ADDITION, THE WATER MAINS SHALL BE TESTED FOR PH AT THE SAME SAMPLE POINTS AND UPSTREAM. TESTING SHALL BE PREFORMED BY A NEW YORK STATE CERTIFIED LABORATORY. BIOLOGICAL AND PH TEST RESULTS OF THE NEWLY CLEANED WATER MAINS SHALL BE SUBMITTED TO, AND APPROVED IN WRITING BY, THE WESTCHESTER COUNTY DEPARTMENT OF HEALTH PRIOR TO PLACING THE MAIN INTO SERVICE.				LICE	೧	VS APP.BY
REASE IN	1.	THE CONTRACTOR SHALL NOTIFY THE VILLAGE ENGINEER, SUPERINTENDENT OF WATER AND SEWER AND THE ENGINEER 48 HOURS PRIOR TO TESTING THE SERVICE LINES.					NUE	CHK.BY CWS
SUPPLY	2.	THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY FOR ANY AND ALL PIPES TAPS REQUIRED FOR TESTING AS SPECIFIED.			MAIN			DSN.BY JMZ
ND SEWER EACH OF	3.	ALL NEW WATER INSTALLATIONS SHALL BE HYDROSTATICALLY TESTED PER A.W.W.A. STANDARD C600 (LATEST VERSION) AT A PRESSURE OF 150PSI, OR 1.5 TIMES THE WORKING PRESSURE OF THE SYSTEM AS A MINIMUM. ALLOWABLE LEAKAGE OF THE TEST SHALL BE AS DEFINED IN SECTION 5.2 OF REFERENCED A.W.W.A. SPECIFICATIONS.		X	OF WATER N			DR.BY JMZ
onded by		ALLOWABLE LEAKAGE WHERE:		NEW YORK	LINING 0		_	0417
		FOR WATER MAIN $L = \frac{SD\sqrt{P}}{148,000}$ $L = ALLOWABLE LEAKAGE, (GPH)$ S = LENGTH OF PIPE TESTED, (FT) D = NOMINAL DIAMETER, (IN) P = AVERAGE TEST, (PSI)		YORKTOWN, NE	MORTAR LIN		UNA C	JOB NO. ENG21–(
		THE ENGINEER REPRESENTING THE TOWN OF YORKTOWN MUST WITNESS THE TESTING OR REVIEW THE RESULTS AND APPROVE THE SYSTEM FOR USE. PRIOR TO COMPLETION OF CONTRACT, RESULTS OF ALL LEAKAGE TESTING SHALL BE		YORK'	IT MOI			
TO BE DISTRICT 10588.	Э.	SUBMITTED TO THE WESTCHESTER COUNTY DEPARTMENT OF HEALTH ALONG WITH ENGINEERS CERTIFICATE OF COMPLETION. TEMPORARY WATER SERVICE NOTES:		TOWN OF	AND CEMENT		ABBRE VIA IIUNS	CONTRACT:
	1.	TEMPORARY WATER SERVICE TO BE INSTALLED PER AWWA STANDARDS AND SHALL BE DISINFECTED AND APPROVED BY THE WCDOH PRIOR TO BEING INSTATED.		•	NG AI		ם מ	NOTED
	2.	AFTER DISINFECTION OF THE TEMPORARY WATER MAINS, OR PORTIONS THEREOF, THE TEMPORARY WATER MAINS SHALL BE FILLED WITH FRESH WATER AND TWO (2) BIOLOGICAL SAMPLES WILL BE COLLECTED AT AN INTERVAL OF 24 HOURS AT THE SAME LOCATION. IN ADDITION, THE TEMPORARY WATER MAINS SHALL BE TESTED FOR PH AT THE SAME SAMPLE POINTS AND UPSTREAM. TESTING SHALL BE PREFORMED BY A NEW YORK STATE CERTIFIED LABORATORY. BIOLOGICAL AND PH TEST RESULTS OF THE TEMPORARY WATER MAINS SHALL BE SUBMITTED TO, AND APPROVED IN WRITING BY, THE WESTCHESTER COUNTY DEPARTMENT OF HEALTH PRIOR TO PLACING THE TEMPORARY MAINS INTO SERVICE.			CLEANING		`	G-1 SCALE:
	3.	TEMPORARY WATER SERVICE MAIN CONNECTION LOCATIONS SHOWN ON THE DRAWINGS IN A PHASED APPROACH. TEMPORARY WATER SERVICE ROUTING SHOWN IS SCHEMATIC ONLY. ACTUAL LAYOUT SHALL BE REFLECTED IN CONTRACTOR'S DETAILED PLAN. CONTRACTOR TO PROVIDE A DETAILED PLAN PROPOSED STREET CROSSINGS AND CONNECTIONS BETWEEN CONNECTION LOCATIONS FOR APPROVAL OF THE ENGINEER.						CADD NO.
		LINE VALVES TO BE PROVIDED EVERY 1,000 LF (MIN.). TEMPORARY HYDRANTS TO BE PROVIDED AT EACH LOCATION WHERE AN EXISTING						
		HYDRANT IS OUT OF SERVICE AS A RESULT OF THE WORK. BASED ON AVAILABLE RECORDS, BUILDINGS SHOWN WITH GRAY HATCH ARE BELIEVED			G			NO.
		TO BE CONNECTED TO A WATER MAIN THAT IS TO BE CLEANED AND CEMENT LINED. THE CONTRACTOR IS TO VERIFY ALL BUILDINGS THAT ARE CONNECTED TO WATER MAIN THAT IS TO BE CLEANED AND RELINED AND IS RESPONSIBLE FOR PROVIDING TEMPORARY WATER SERVICE TO ALL AFFECTED BUILDINGS FOR THE ENTIRE DURATION OF THE WORK.			EE	T 2	 OF	FILE
		SOLITIGHT ZUZZ WESTUN & SAMESUN	I L					-

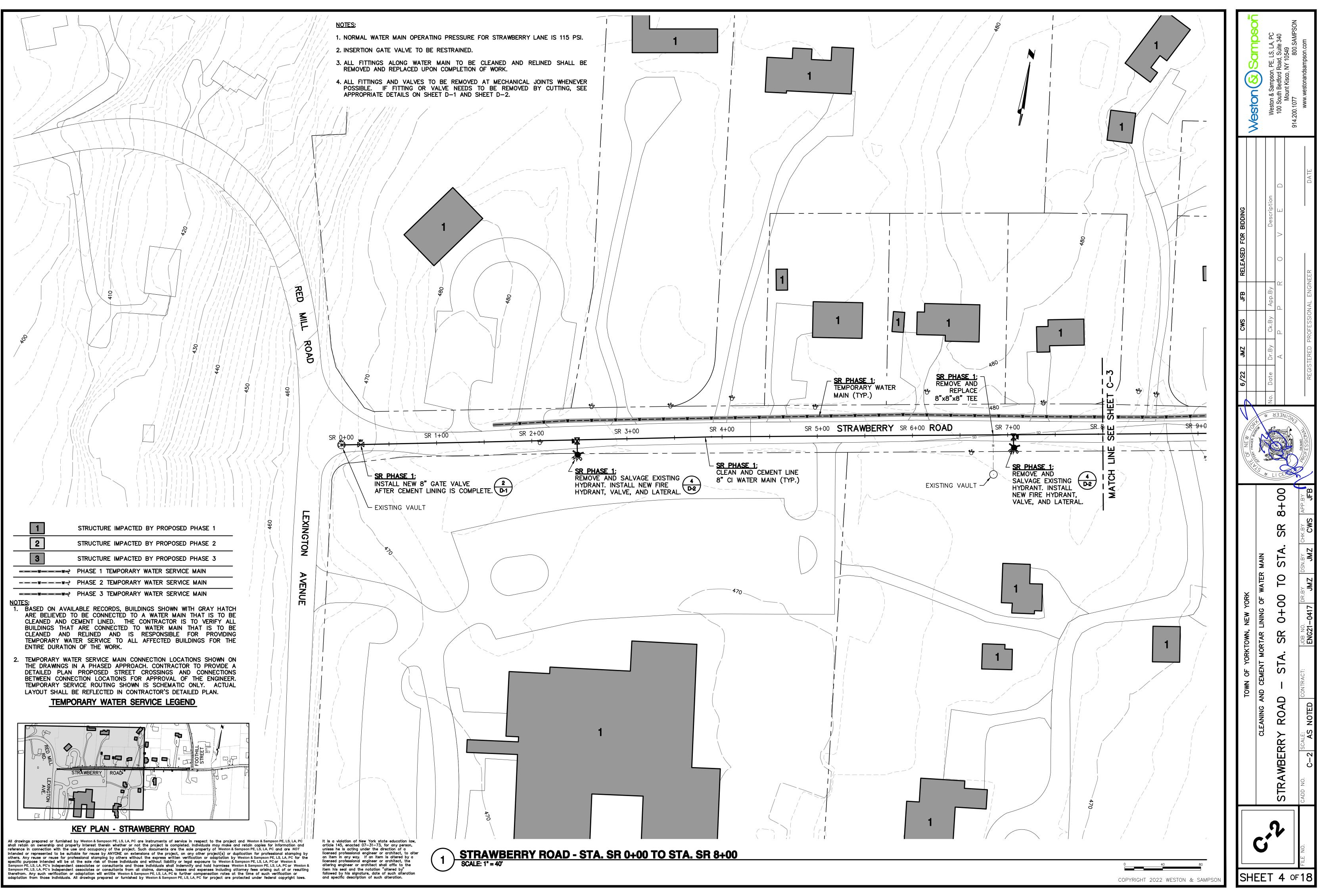


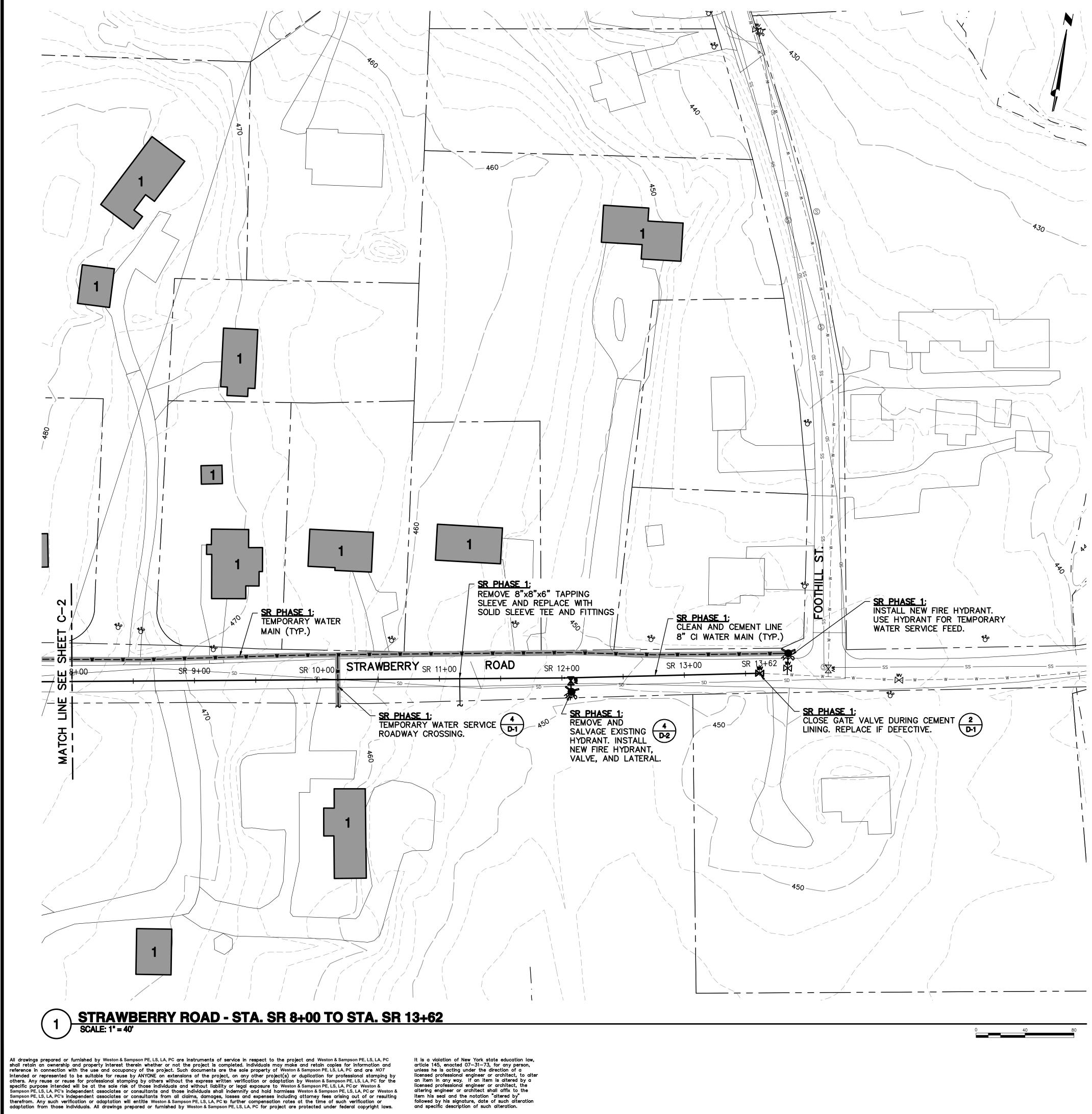


All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are *NOT* intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC for the specific purpose intended will be at the sole risk of those individuals and without liability or legal exposure to Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants and those individuals shall indemnify and hold harmless Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.



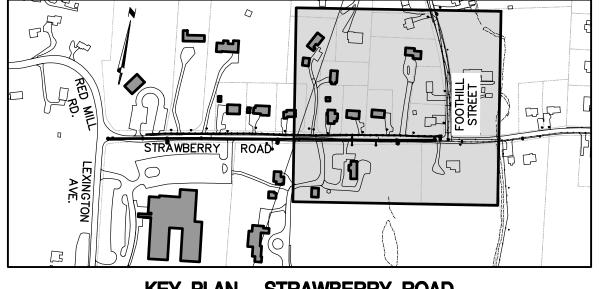




item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.

NOTES:

- 1. NORMAL WATER MAIN OPERATING PRESSURE FOR STRAWBERRY LANE IS 115 PSI.
- 2. INSERTION GATE VALVE TO BE RESTRAINED.
- 3. ALL FITTINGS ALONG WATER MAIN TO BE CLEANED AND RELINED SHALL BE REMOVED AND REPLACED UPON COMPLETION OF WORK.
- 4. ALL FITTINGS AND VALVES TO BE REMOVED AT MECHANICAL JOINTS WHENEVER POSSIBLE. IF FITTING OR VALVE NEEDS TO BE REMOVED BY CUTTING, SEE APPROPRIATE DETAILS ON SHEET D-1 AND SHEET D-2.



KEY PLAN - STRAWBERRY ROAD

1	STRUCTURE IMPACTED BY PROPOSED PHASE 1
2	STRUCTURE IMPACTED BY PROPOSED PHASE 2
3	STRUCTURE IMPACTED BY PROPOSED PHASE 3
	PHASE 1 TEMPORARY WATER SERVICE MAIN
ww-	PHASE 2 TEMPORARY WATER SERVICE MAIN
ww_?	PHASE 3 TEMPORARY WATER SERVICE MAIN

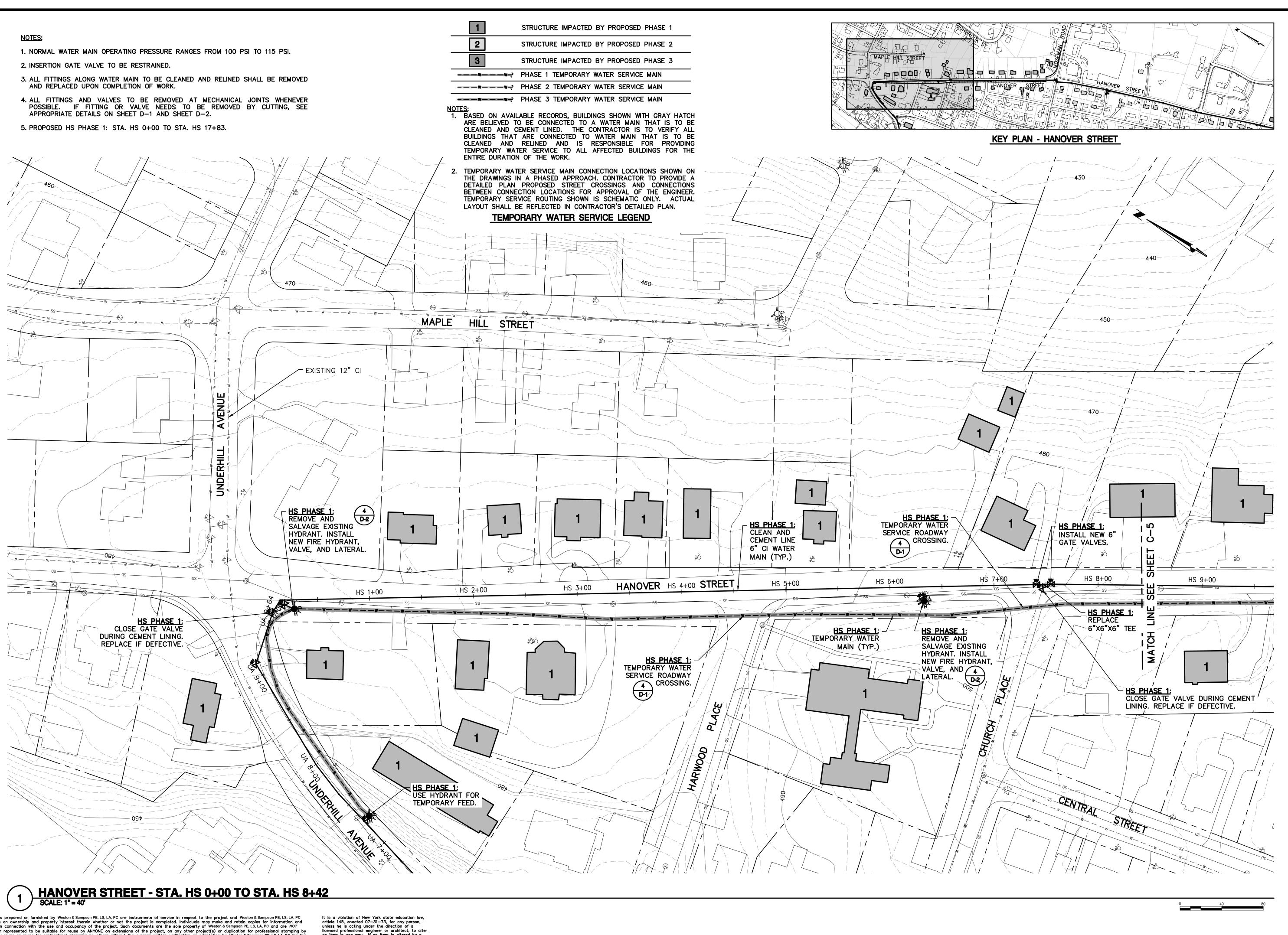
- NOTES: 1. BASED ON AVAILABLE RECORDS, BUILDINGS SHOWN WITH GRAY HATCH TO A WATER MAIN THAT IS TO BE ARE BELIEVED TO BE CONNECTED TO A WATER MAIN THAT IS TO BE CLEANED AND CEMENT LINED. THE CONTRACTOR IS TO VERIFY ALL BUILDINGS THAT ARE CONNECTED TO WATER MAIN THAT IS TO BE CLEANED AND RELINED AND IS RESPONSIBLE FOR PROVIDING TEMPORARY WATER SERVICE TO ALL AFFECTED BUILDINGS FOR THE ENTIRE DURATION OF THE WORK.
- 2. TEMPORARY WATER SERVICE MAIN CONNECTION LOCATIONS SHOWN ON THE DRAWINGS IN A PHASED APPROACH. CONTRACTOR TO PROVIDE A DETAILED PLAN PROPOSED STREET CROSSINGS AND CONNECTIONS BETWEEN CONNECTION LOCATIONS FOR APPROVAL OF THE ENGINEER. TEMPORARY SERVICE ROUTING SHOWN IS SCHEMATIC ONLY. ACTUAL LAYOUT SHALL BE REFLECTED IN CONTRACTOR'S DETAILED PLAN.

TEMPORARY WATER SERVICE LEGEND

S						NF 4	6/22	ZML	CWS	JFB	RELEASED FOR BIDDING	
H		IOWN OF TORKIOWN, NEW TORK	UWN, NEW TURK			C NEW LO						Weston & Sampeo
ΞΕ	C	CLEANING AND CEMENT MORTAR LINING OF WATER MAIN	TAR LINING OF WATER MA	N		S ter mont						
T							No. Date	Dr.By	Ck.By	App.By	Description	Weston & Sampson, PE, LS, LA, PC
5	3	STRAWBERRY ROAD – STA. SR 8+00 TO STA. SR 13+6	SR 8+00 TO S1	A. SR	13+62	AEEL ADDI		A		R	O V E D	100 South Bedford Road, Suite 340
OF))									Mount Kisco, NY 10549 914.200.1077 800.SAMPSON
	file no.	CADD NO. SCALE: CONTRACT: U	JOB NO. DR.BY DSN.BY CHK.BY APP.BY ENG21-0417 JMZ JMZ CWS UF	зү снк.вү JMZ сw	S APP.BY JFB	POFESSIONAL	REC	GISTERED	REGISTERED PROFESSIONAL ENGINEER	AL ENGINE	ER DATE	

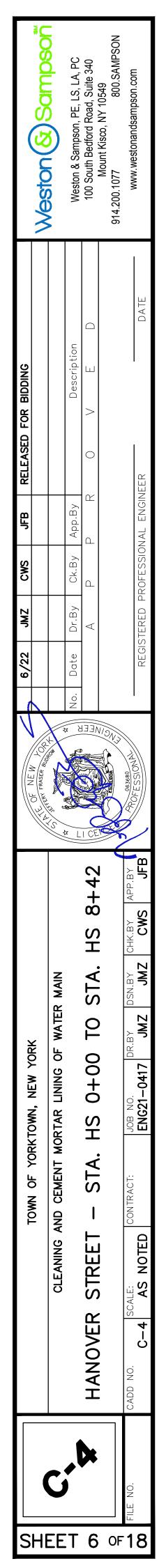
30

- APPROPRIATE DETAILS ON SHEET D-1 AND SHEET D-2.

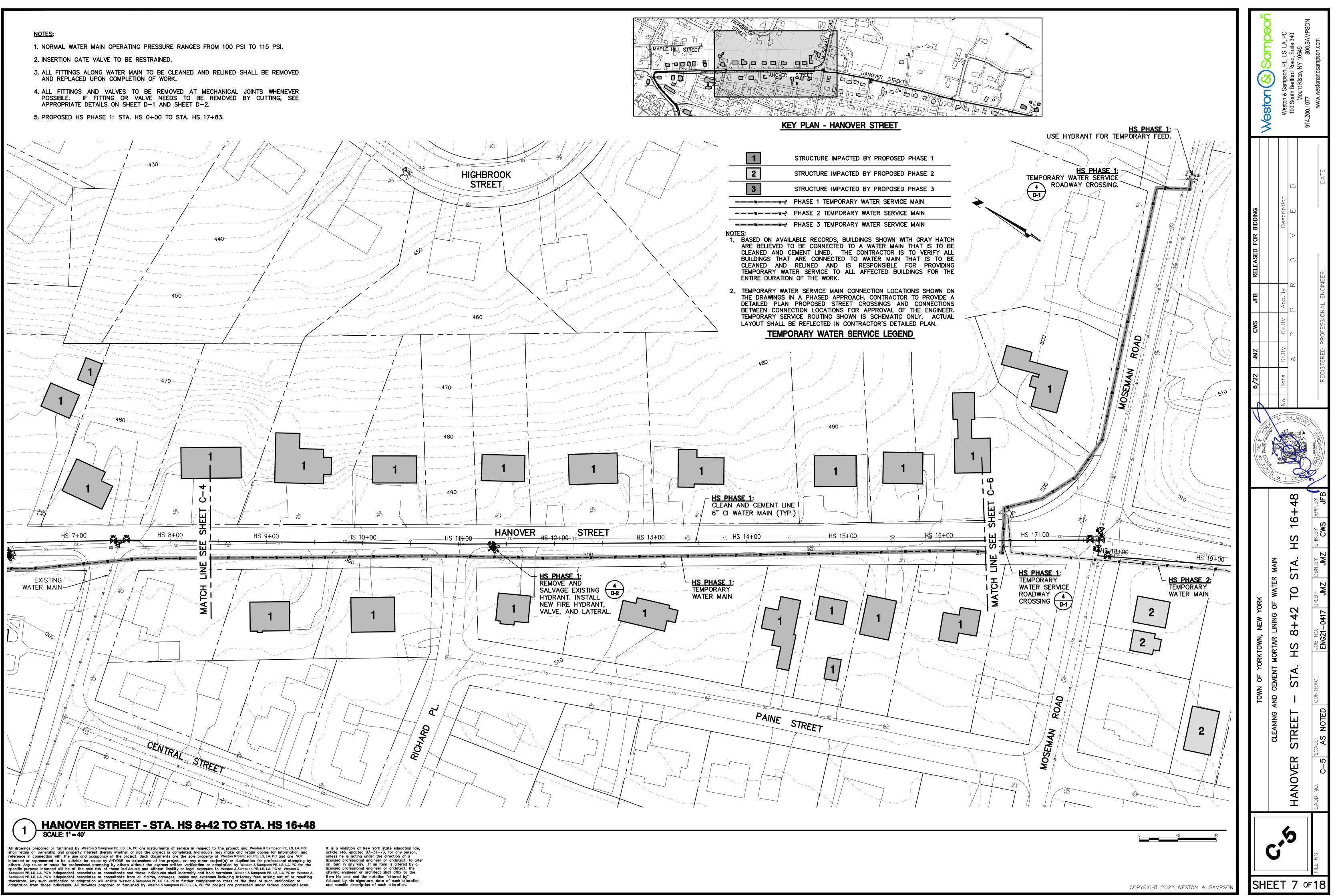


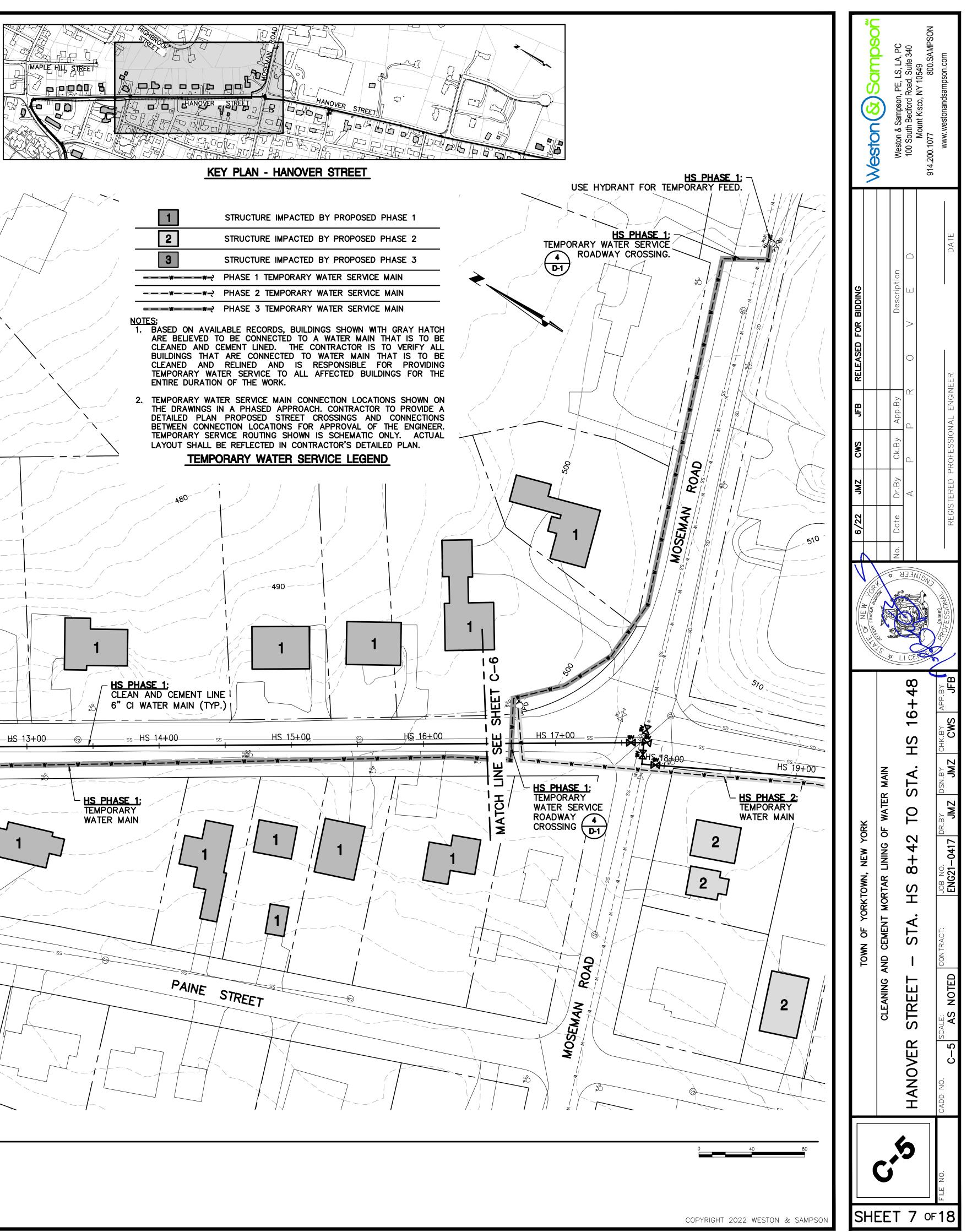
All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are *NOT* intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC for the specific purpose intended will be at the sole risk of those individuals and without liability or legal exposure to Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants and those individuals shall indemnify and hold harmless Weston & Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

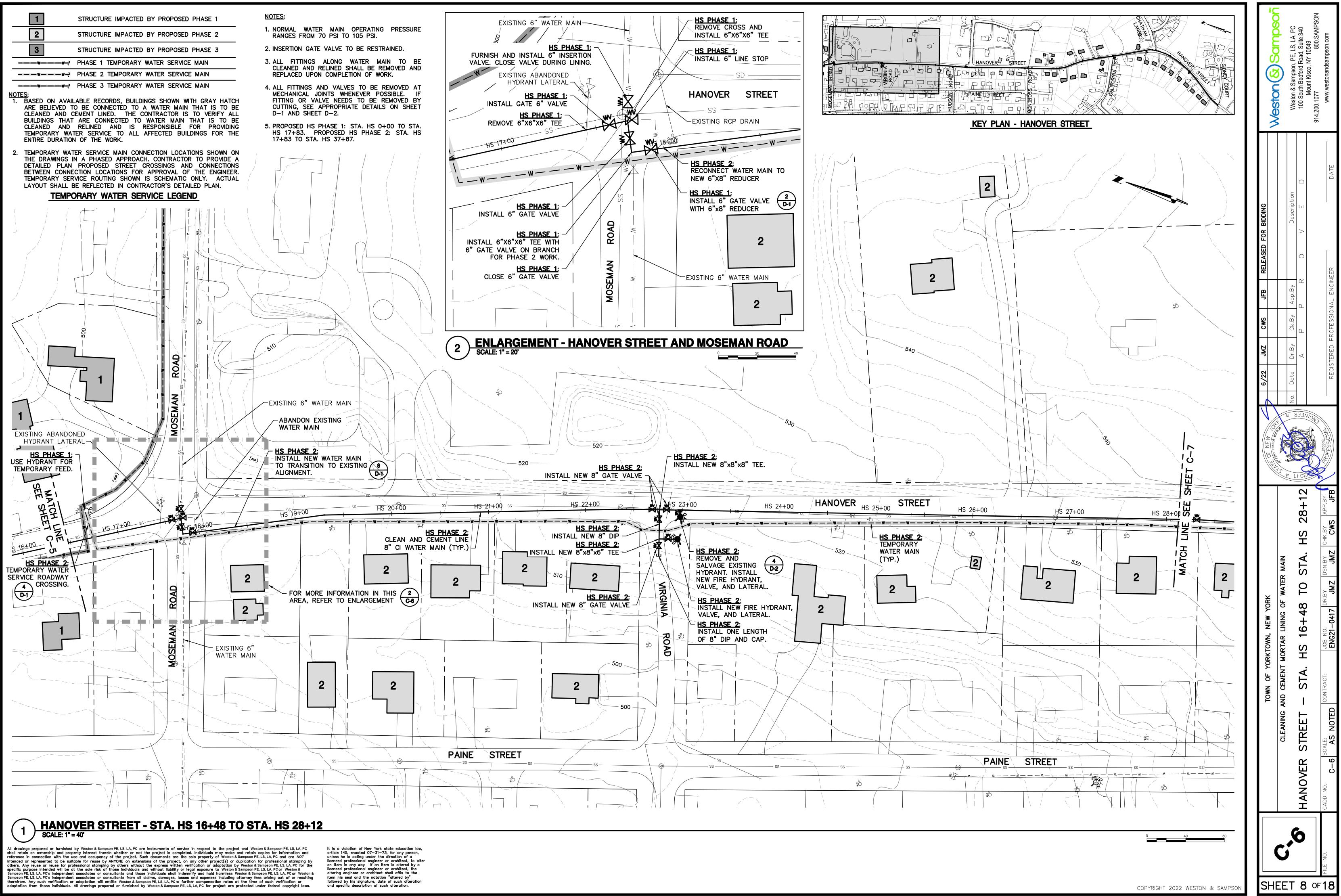
It is a violation of New York state education law, article 145, enacted 07-31-73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a licensed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.



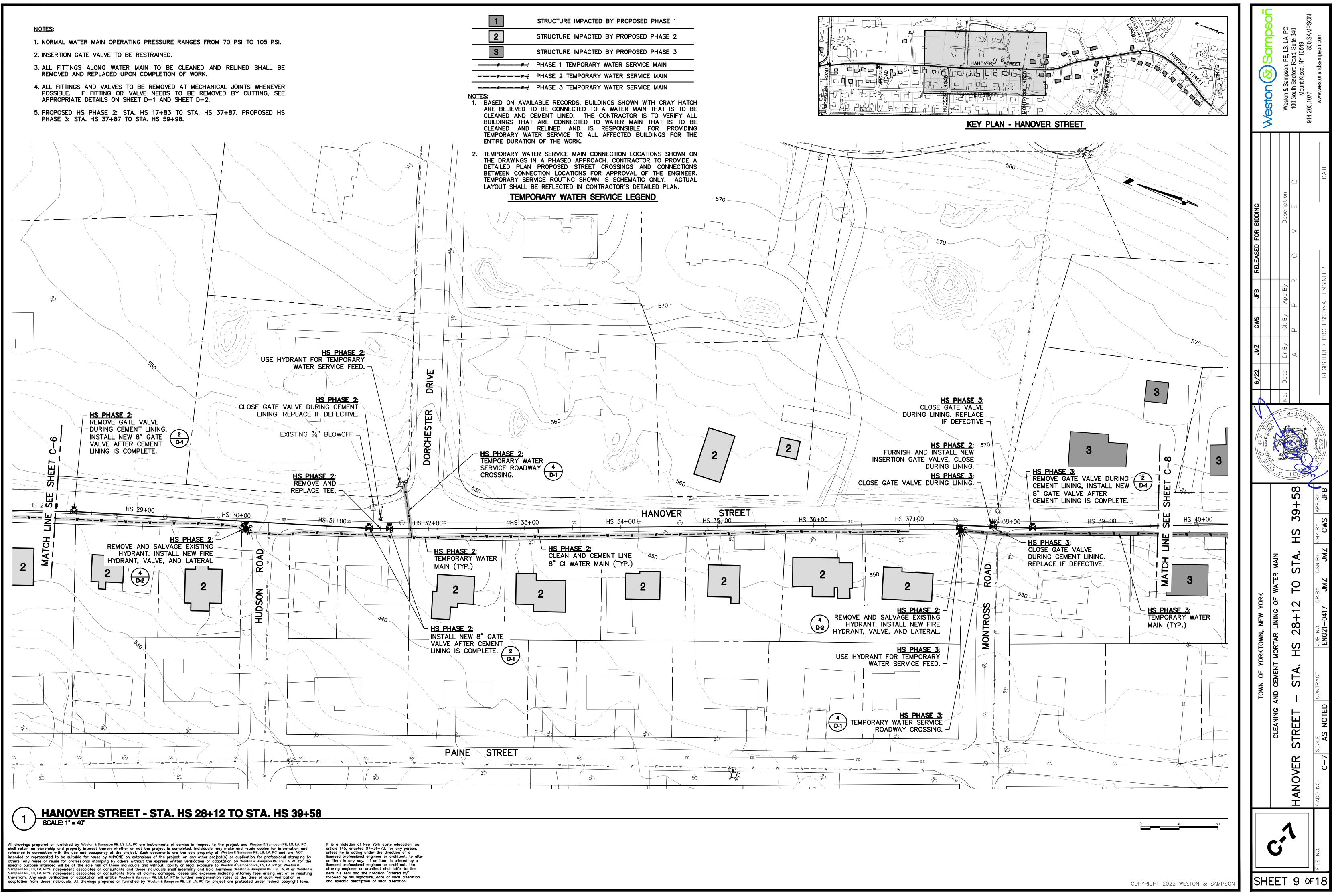
- APPROPRIATE DETAILS ON SHEET D-1 AND SHEET D-2.



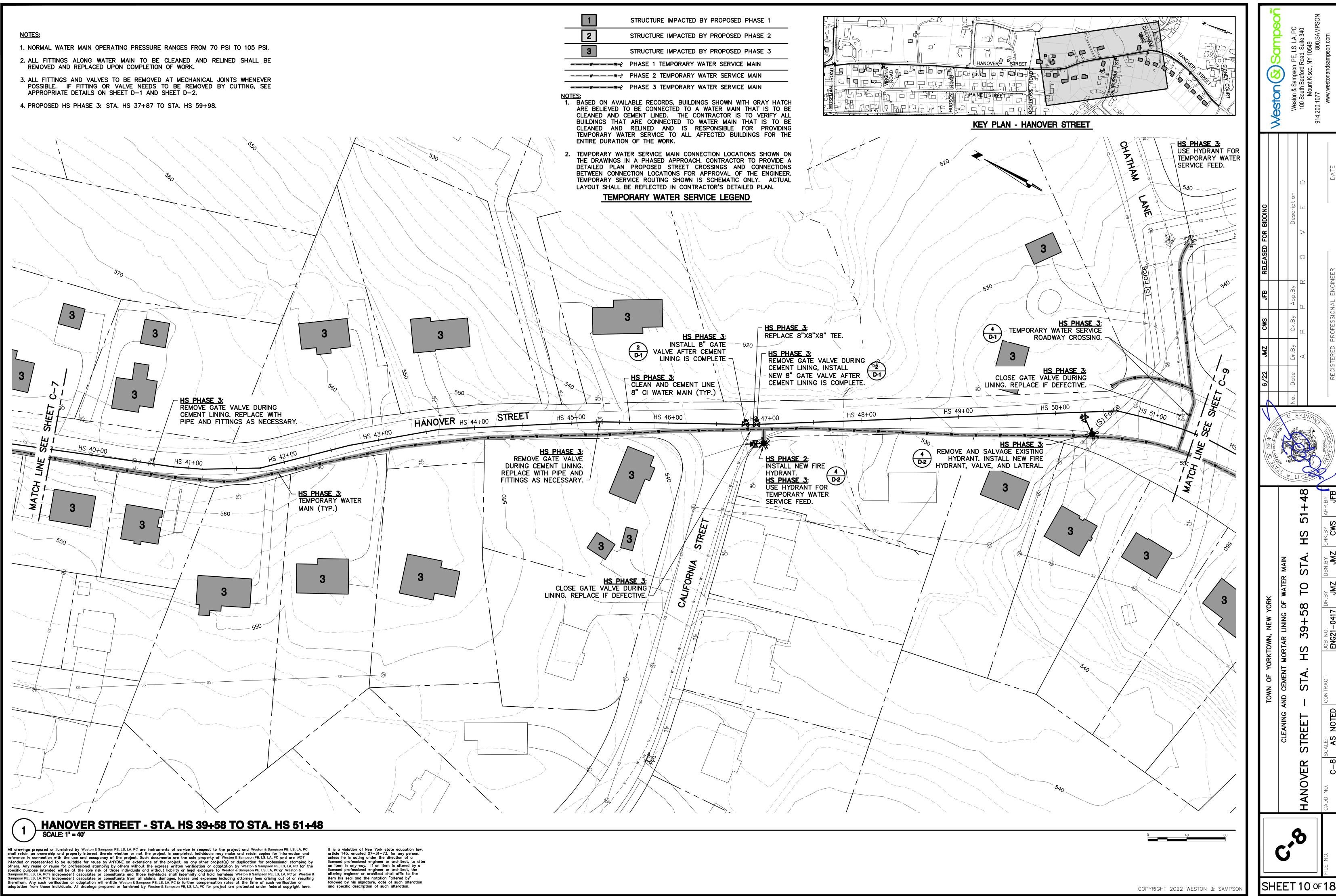




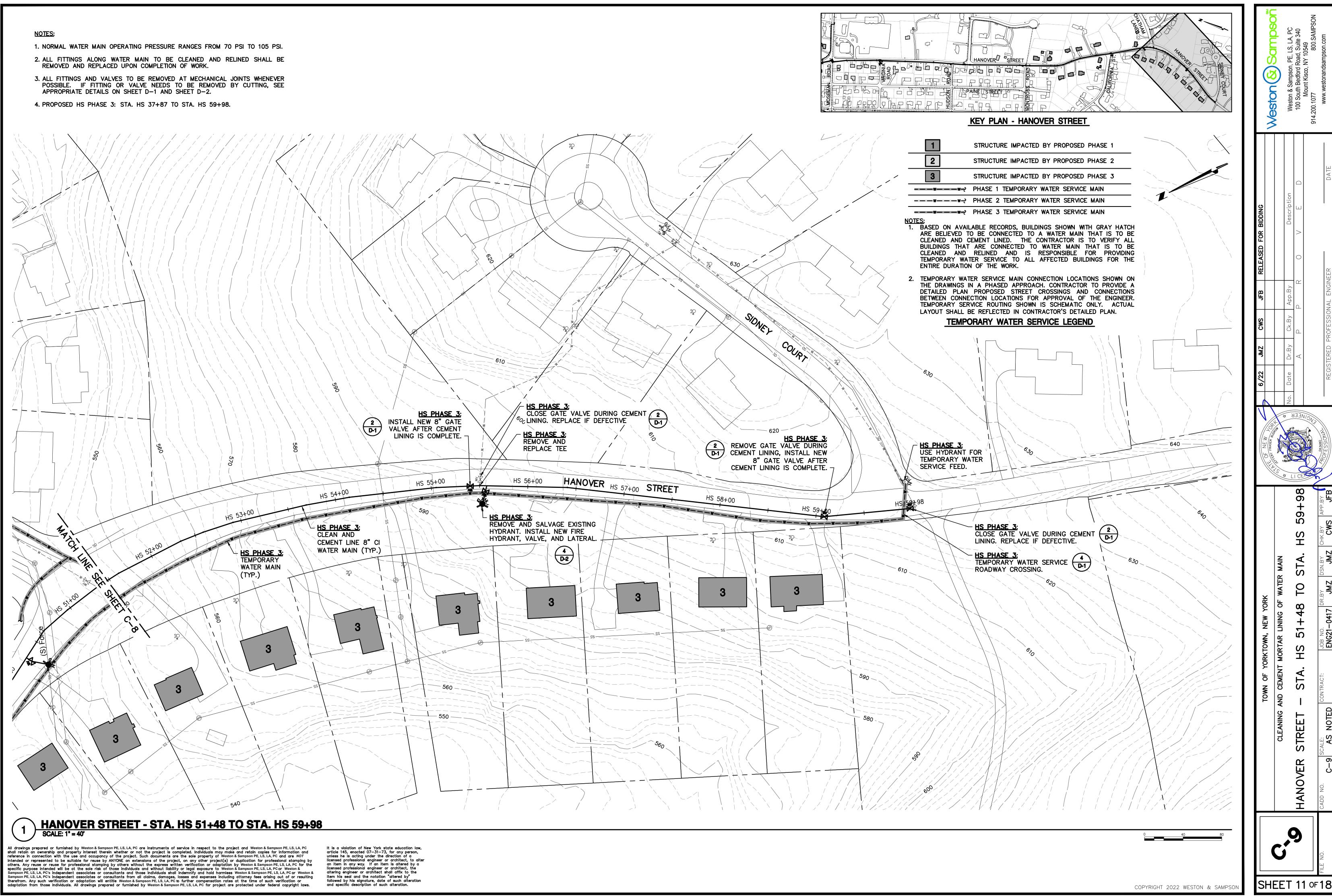
- APPROPRIATE DETAILS ON SHEET D-1 AND SHEET D-2.
- PHASE 3: STA. HS 37+87 TO STA. HS 59+98.

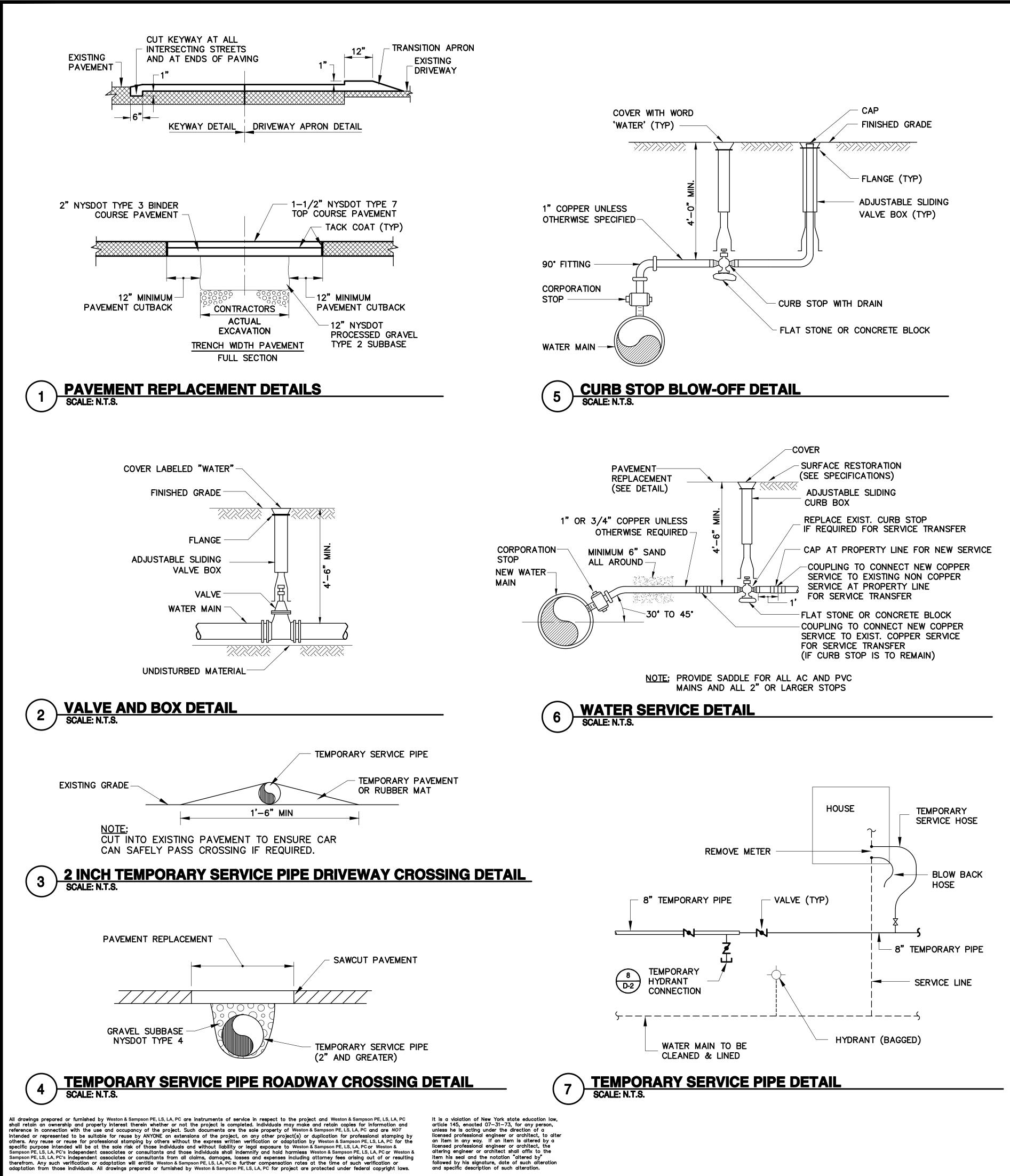


- REMOVED AND REPLACED UPON COMPLETION OF WORK.
- APPROPRIATE DETAILS ON SHEET D-1 AND SHEET D-2.



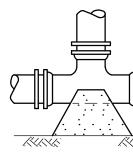
- APPROPRIATE DETAILS ON SHEET D-1 AND SHEET D-2.





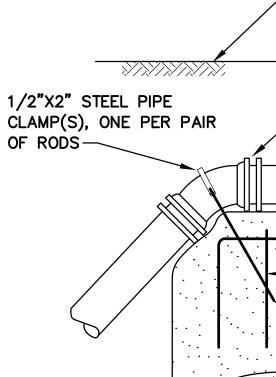


FEET	A
SIZE OF MAIN	
6", 8"	
10",12"	
<u>NOTE</u> :	
	SIZE OF MAIN 6", 8" 10", 12"



TEE DETAIL

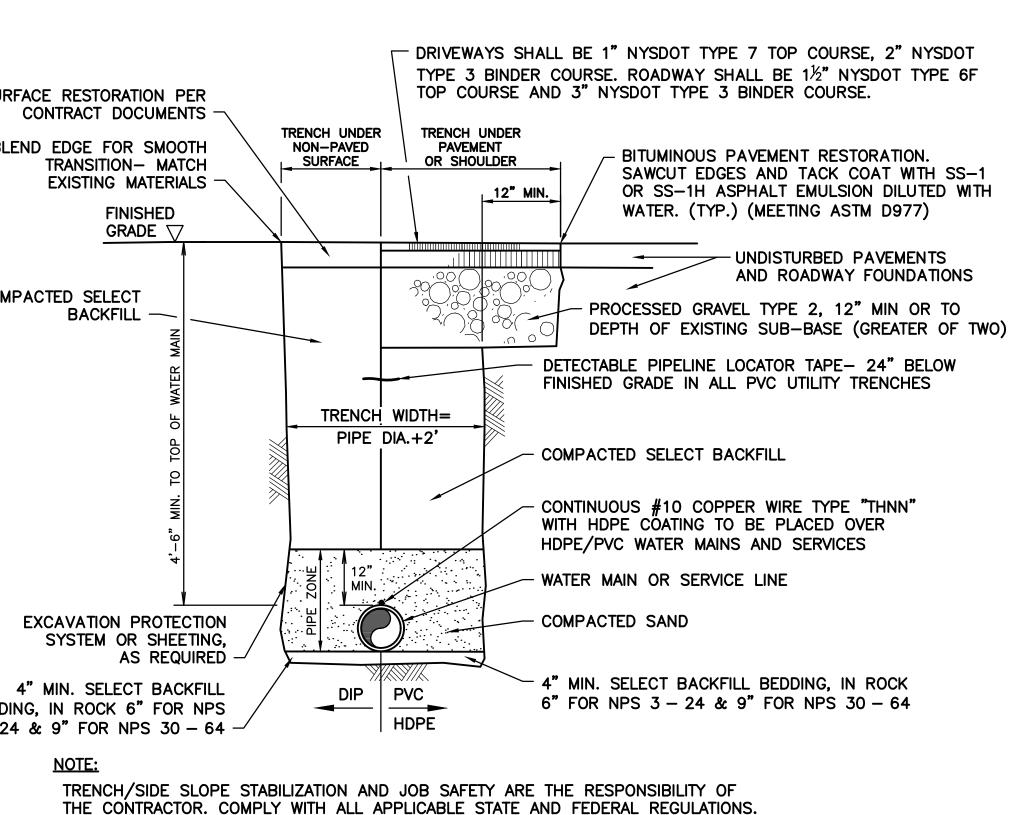
(PLAN VIEW)

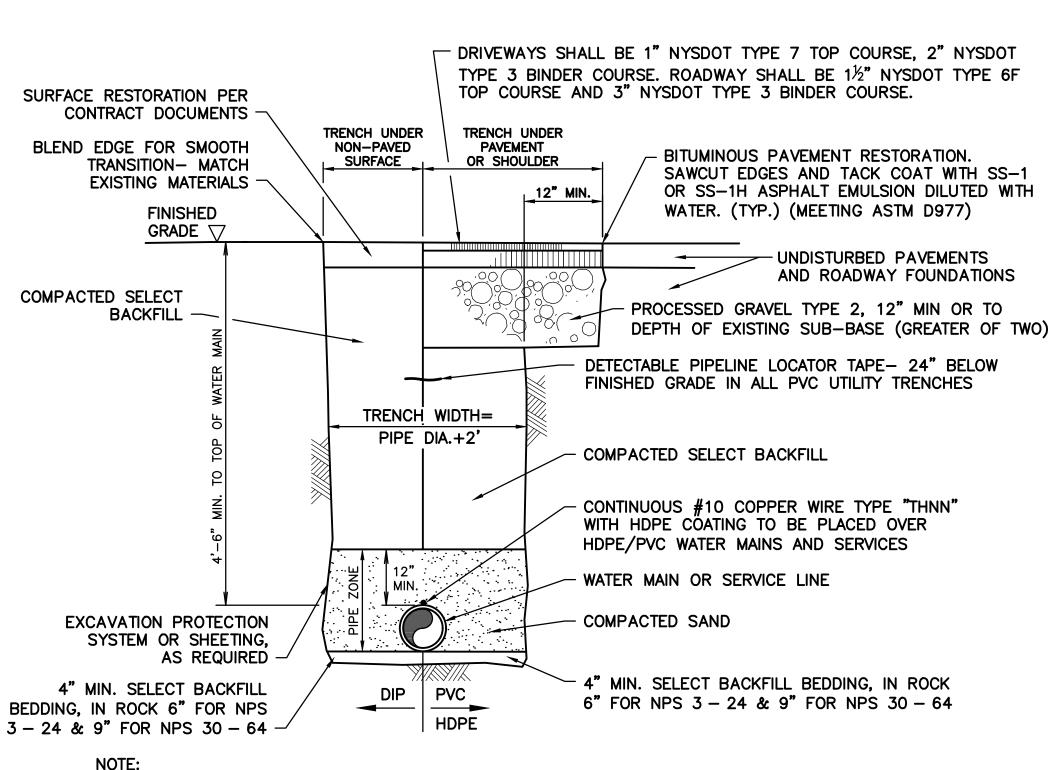


UNDISTURBED MATERIAL

8

SCALE: N.T.S.





TYPICAL WATER MAIN TRENCH DETAIL

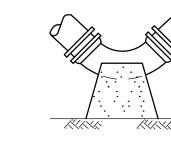


GRADE

NO. AND SIZE OF Table Of Dimensions -BEND IN VERTICAL THREADED RODS For Anchor Blocks PLANE NO. DIAM. BEND SIZE Volume 6"-22.5* 1/2" 0.6 C.Y. 2 1/2" 6**"**-45° 1.0 C.Y. 2 8"-22.5° 1.0 C.Y. 1/2" 8"-45* 1.8 C.Y. 2 3/4" 12**"**-22.5° 2.5 C.Y. 2 3/4" #4U @12 4 3/4" 12"45 4.0 C.Y. #4U @12

> THREADED STEEL ROD 24" MIN. EMBEDMENT. FOR NUMBER AND DIAMETER, SEE TABLE CONCRETE ANCHOR BLOCK

(VOLUME AS NOTED IN TABLE)



CAP DETAIL (PLUG SIMILAR) (SECTION VIEW)

1'-6"

BEARING AREA

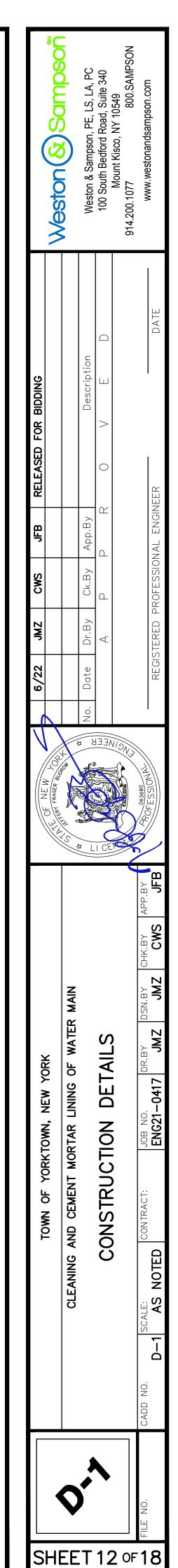
AS SHOWN IN

TABLE (TYP)

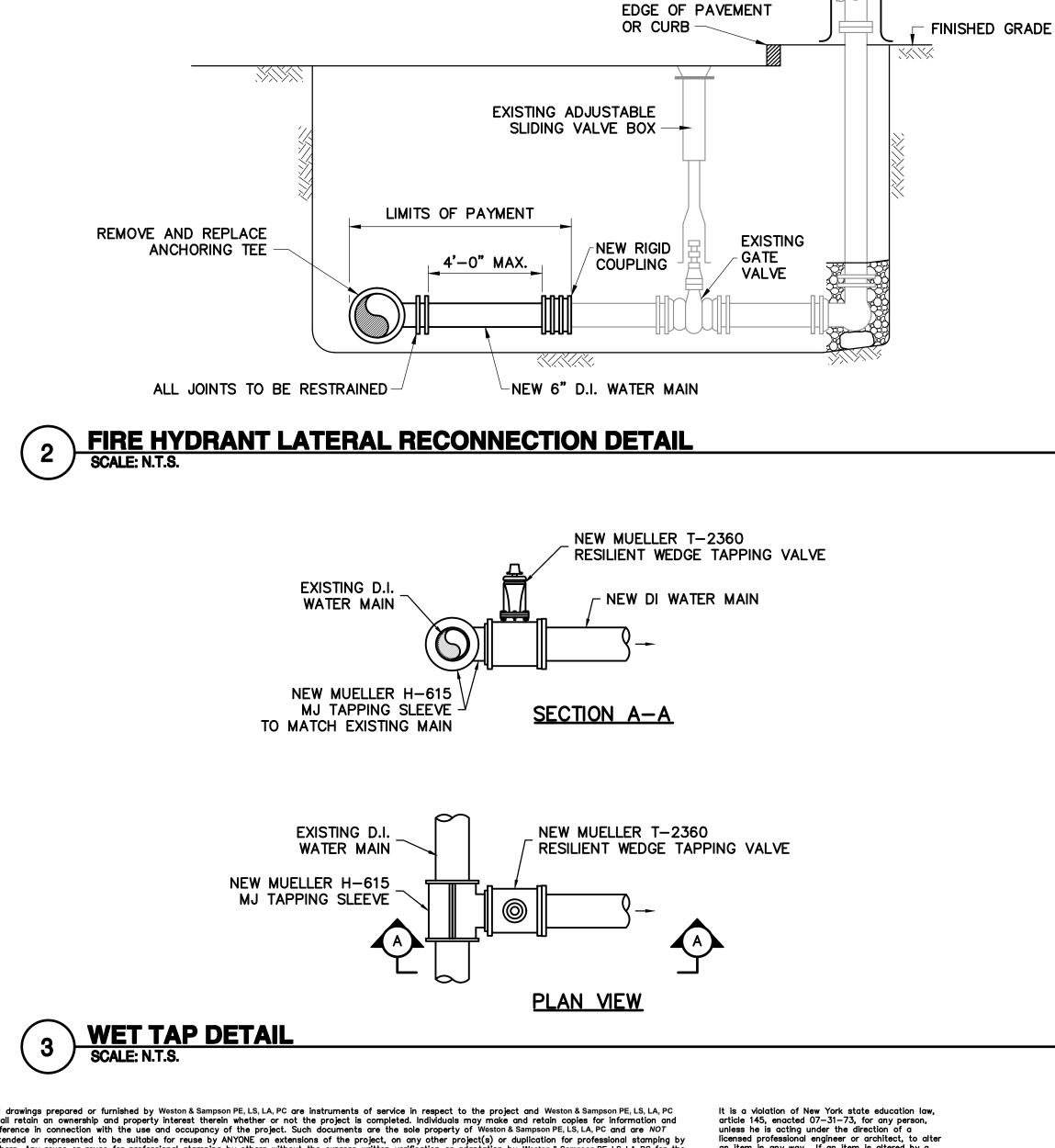


	NCRETE THRUST F GAINST UNDISTUF			
SIZE OF MAIN	90° BENDS, TEES, CAPS AND PLUGS	45° BENDS AND WYES	22-1/2° BENDS	11-1/4° BENDS
6", 8"	5	4	2	2
10", 12"	12	9	5	2

1. CONTRACTOR SHALL USE CARE TO AVOID PLACEMENT OF CONCRETE ON THE FITTING JOINTS

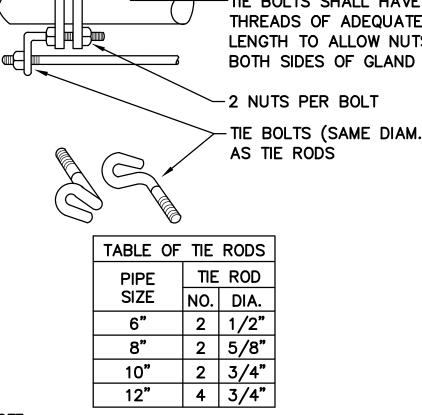


It is a violation of New York state education law, article 145, enacted 07-31-73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a licensed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.



TIE ROD JOINT RESTRAINT SHALL ONLY BE USED WHERE OTHER MEANS OF RESTRAINT ARE NOT FEASIBLE.

) JOINT RESTRAINT TIE RODS DETAIL SCALE: N.T.S.



NOTE:

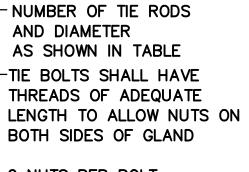


TABLE 1 REQUIRED LENGTH OF RESTRAINED JOINTS FROM FITTINGS (FEET)

PIPE SIZE	90° BEND	45° BEND OR WYE BRANCH	22 1/2° BEND	11 1/4° BEND	PLUG OR CAP	TEE (BRANCH)
6"	25 (30.5)	10.5 (12.5)	5 (6)	2.5 (3)	43 (64)	34 (51)
8"	33 (40)	13.5 (16.5)	6.5 (8)	3 (4)	55 (82)	47 (70)
10"	40 (48.5)	16.5 (20)	8 (9.5)	4 (5)	67 (100)	58 (87)
12"	47 (56.5)	19.5 (23.5)	9.5 (11.5)	4.5 (5.5)	79 (118)	70 (105)
16"	59.5 (72)	24.5 (30)	12 (14.5)	6 (7)	101 (152)	92 (139)
20"	72 (86.5)	30 (36)	14.5 (17)	7 (8.5)	123 (184)	114 (171)
24"	84 (100)	35 (41)	16.5 (20)	8 (10)	144 (216)	134 (202)
30"	100 (120)	41 (50)	20 (24)	10 (12)	174 (261)	165 (247)

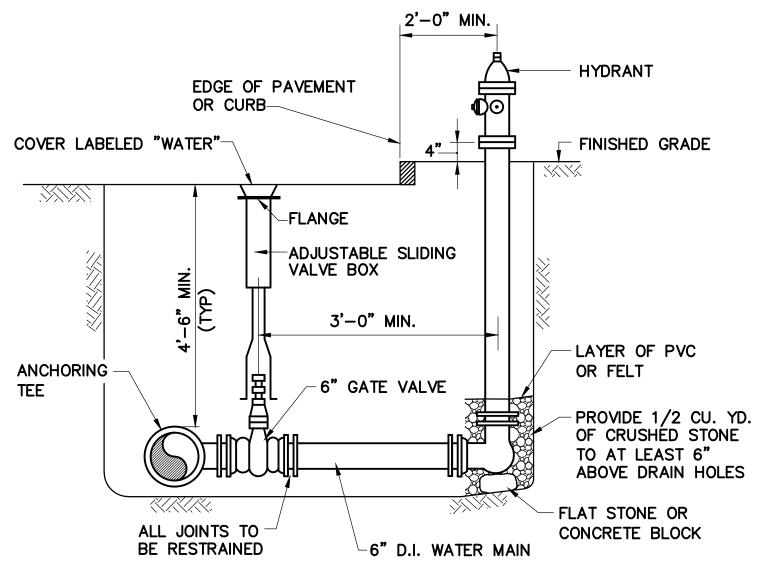
NOTES:

1. RESTRAINED LENGTHS LISTED IN PARENTHESES ARE FOR PIPE WRAPPED IN POLYETHYLENE. THE OTHER ASSOCIATED LENGTHS ARE FOR PLAIN UNWRAPPED DUCTILE IRON PIPE.

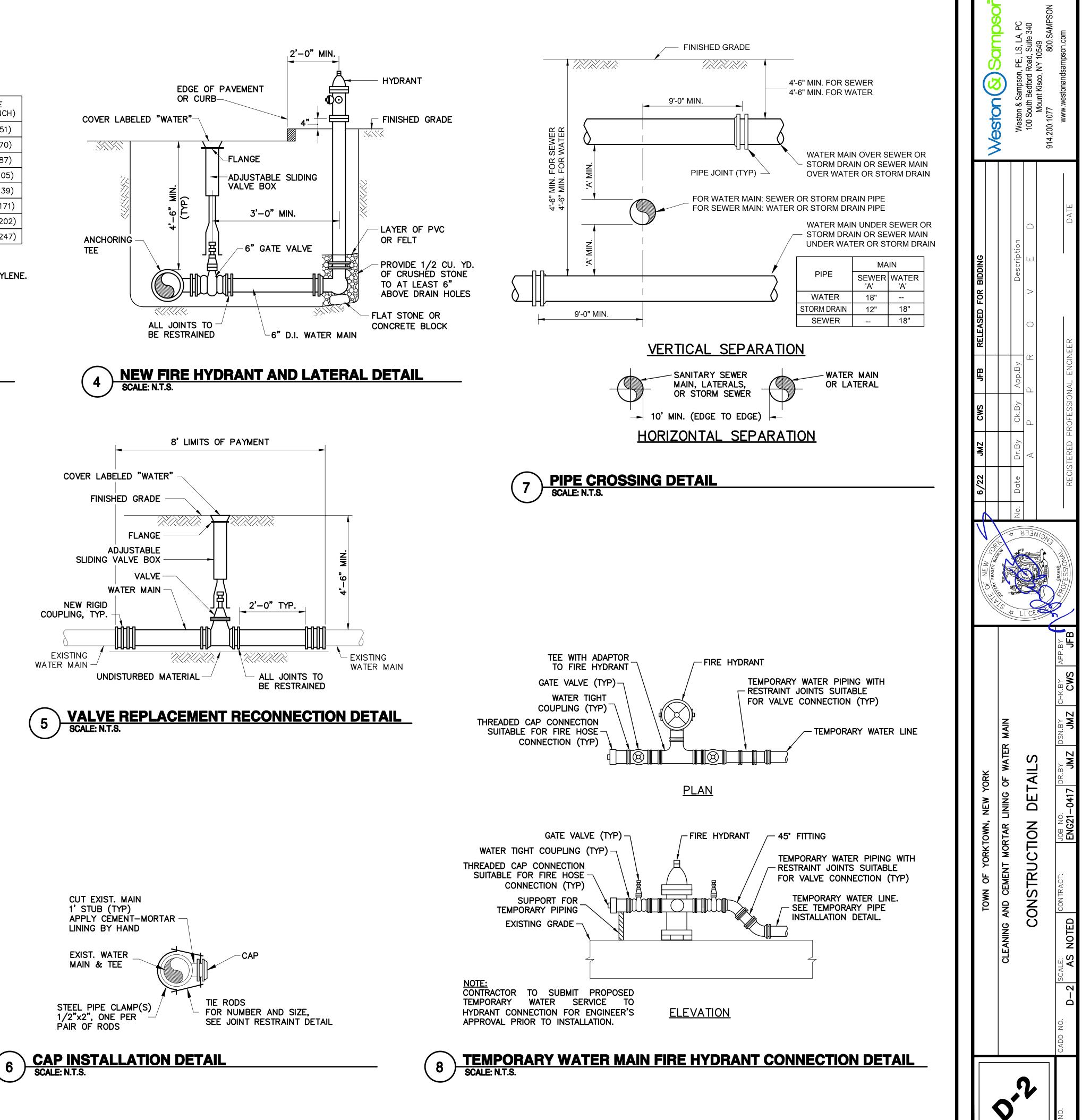
2. THE CONTRACTOR SHALL USE THIS TABLE IN CONJUNCTION WITH THE APPROPIATE PIPE SPECIFICATION SECTION.

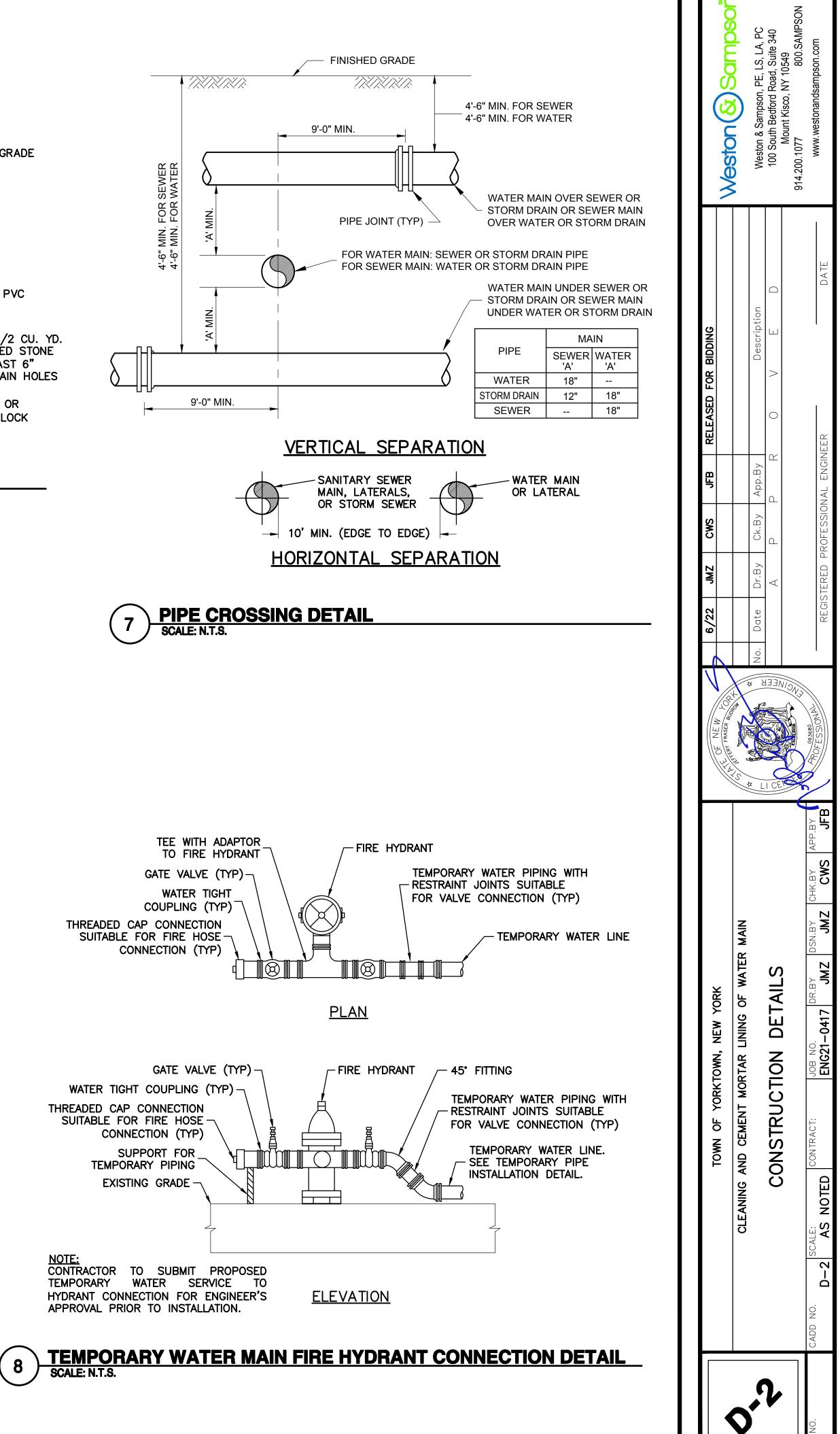
BURLAP BAG OVER

HYDRANT WHILE OUT OF SERVICE









COPYRIGHT 2022 WESTON & SAMPSON

SHEET 13 OF 18

SUGGESTED EROSION CONTROL CONSTRUCTION SCHEDULE:

- AREA BEYOND THE GRADING LIMITS FOR PROTECTION. 2. INSTALL TEMPORARY CONSTRUCTION ENTRANCE AT APPROXIMATE LOCATION OF DRIVEWAY IF DRIVEWAY
- STONE HAS NOT YET BEEN PLACED. INSTALL PROTECTIVE MEASURES AROUND TREES TO BE
- RETAINED WITHIN GRADING LIMITS. INSTALL BRIGHTLY COLORED CONSTRUCTION FENCE ALONG ROAD TO LIMIT VEHICULAR ACCESS TO STONE
- DRIVEWAY OR CONSTRUCTION ACCESS DRIVE. INSTALL INLET PROTECTION DEVICES AT CATCH BASINS DOWN SLOPE FROM THE SITE THAT ARE VULNERABLE
- TO SEDIMENT ACCUMULATIONS. IN DESIGNATED AREAS.
- INSTALL SILT FENCES IN LOCATIONS AROUND THE PERIMETER OF SITE WORK, STOCKPILE AREA AND ALONG THE CONTOUR OF ALL DISTURBED SLOPES AT A MINIMUM OF EVERY 50' OF HORIZONTAL DISTANCE OR AS SPECIFIED, MEASURED PERPENDICULAR TO THE SLOPE.
- ROUGH GRADE SWALES AROUND PROPOSED EARTHWORK AND STRUCTURES TO EXTENT POSSIBLE WITHIN GRADING LIMITS, INSTALL SILT FENCES, STRAW BALE DIKES MEASURES AS SHOWN ON PLANS, AND AS NECESSARY TO COMPLY WITH THE SWPPP AND ENSURE WATER QUALITY OF RUNOFF.
- CONTRACTOR MUST ROUTINELY INSPECT AND MAINTAIN EROSION CONTROL DEVICES AND BEST MANAGEMENT SEPARATE CONTRACTOR'S LOG.
- 0. ROUTE ALL DEWATERING AND SUMP PUMP OUTFALLS, OF TURBID QUALITY, DIRECTLY TO SEDIMENT BASINS OR OTHER APPROPRIATE BMP.
- THE CONTRACTOR SHALL INITIATE STABILIZATION OF ANY BARE SOIL AREAS, AS SOON AS POSSIBLE, BUT IN NO CASE MORE THAN 14 DAYS AFTER INITIAL DISTURBANCE OF THE RESPECTIVE AREAS OF THE SITE. THE CONTRACTOR SHALL RETAIN SITE RECORDS OF THE EXCEPTIONS TO THIS POLICY CAN BE GRANTED UNDER NORMAL CONDITIONS IN THE FOLLOWING INSTANCES:
- A. WHERE THE INITIATION OF STABILIZATION MEASURES BY THE 14TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASED IS PRECLUDED BY SNOW COVER OR FROZEN GROUND CONDITIONS, STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE.
- WHERE CONSTRUCTION ACTIVITY ON A PORTION OF THE SITE IS TEMPORARILY CEASED, AND EARTH-DISTURBING ACTIVITIES WILL BE RESUMED WITHIN TWENTY-ONE (21) DAYS, TEMPORARY STABILIZATION MEASURES NEED NOT BE INITIATED ON THAT PORTION OF THE SITE IF REQUESTED IN WRITING AND APPROVED BY THE ENGINEER.
- 12. ALL EROSION CONTROL MEASURES SHALL REMAIN IN PLACE UNTIL FINAL STABILIZATION IS ATTAINED. REMOVAL OF ANY EROSION CONTROL MEASURES MUST FIRST BE APPROVED BY THE ENGINEER AND/OR THE JURISDICTION HAVING AUTHORITY.
- 3. WHEN WEATHER CONDITIONS PROHIBIT SEED GERMINATION, DISTURBED GROUND SHOULD BE MULCHED WITH STRAW OR FIBER MULCH AND RECEIVE BINDER/TACK APPLICATION OR EQUIVALENT.
- 4. THE SCHEDULE DESCRIPTIONS ABOVE ARE SUGGESTIONS PROVIDED TO ASSIST THE CONTRACTOR(S) IN DEVELOPING THEIR STORM WATER POLLUTION PREVENTION PLAN (SWPPP) SCHEDULE SPECIFIC TO THIS PROJECT. THE ACTUAL SCHEDULING AND IMPLEMENTATION OF THE SWPPP AND MAINTENANCE OF REQUIRED WATER QUALITY IS THE RESPONSIBILITY OF THE CONTRACTOR(S). THE EROSION AND SEDIMENT CONTROL PLAN AND DEVICES SHOWN ARE CONSIDERED TO COMPRISE THE MAJORITY OF EFFORTS NEEDED, BUT NOT NECESSARILY ALL THAT WILL BE REQUIRED. WEATHER, SITE AND UNFORESEEN CONDITIONS CAN DICTATE THAT GREATER EFFORTS WILL BE NECESSARY. IN THE CASE OF PROJECTS THAT DISTURB MORE THAN 1 ACRE OF LAND, THE OWNER, OR OWNER'S REPRESENTATIVE, WILL DEVELOP THE SWPPP WITH SUBMITTED CONTRIBUTIONS FROM THE ASSIGNED CONTRACTORS PERFORMING PROJECT SITE WORK. THESE CONTRIBUTIONS WILL CONSIST OF AN EROSION AND CONTROL SCHEDULE (AS SPECIFIED IN THE PROJECT MANUAL), SHORT NARRATIVE OF ANTICIPATED EROSION CONTROL ACTIVITIES, INSPECTION REPORTS AND LOGS SIGNED CERTIFICATION STATEMENTS AND AND PRE-CONSTRUCTION PHOTOGRAPHS AS SPECIFIED. THIS COMPETENT PERSON SHALL BE EITHER A LICENSED ENGINEER, LANDSCAPE ARCHITECT OR CERTIFIED EROSION CONTROL SPECIALIST.

GENERAL MAINTENANCE PLAN:

- . FLAG THE GRADING LIMITS AND MARK A 10' BUFFER 1. ALL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE CHECKED FOR STABILITY AND OPERATION FOLLOWING EVERY RUNOFF PRODUCING RAINFALL, BUT IN NO CASE LESS THAN ONCE EVERY WEEK, IN ACCORDANCE WITH THE SWPPP AND NYSDEC SPDES GENERAL PERMIT No. GP-0-10-01. ANY NEEDED REPAIRS WILL BE MADE IMMEDIATELY TO MAINTAIN ALL PRACTICES AS DESIGNED.
 - 2. SEDIMENT WILL BE REMOVED FROM BEHIND STRAW BALE DIKES AND BEHIND SILT FENCES WHEN IT BECOMES 6" DEEP AT THE DIKE/FENCE OR WHEN ACCUMULATIONS HAVE ADVERSELY AFFECTED IT'S FUNCTION. STRAW BALE DIKES AND SILT FENCES WILL BE REPAIRED BY REMOVING SILT AND SEDIMENTS AND THEN TAMPING LOOSE SOIL ALONG BASE, REPLACING DAMAGED OR WEAKENED POSTS AND STAKES, OR AS NECESSARY TO MAINTAIN A BARRIER.
- COMPLETE SITE CLEARING, STOCKPILE SAVED MATERIALS 3. SEDIMENT WILL BE REMOVED AND FILTER DEVICES CLEANED OR REPLACED AT CATCH BASINS WHEN THE SEDIMENT POOL NO LONGER DRAINS FREELY. SEDIMENT ACCUMULATIONS WITHIN DRAINAGE STRUCTURES AND PIPING SHALL BE CLEANED OUT AT THE PROJECT COMPLETION AND AS ORDERED BY ENGINEER WHEN DETERMINED THAT PRE-COMPLETION INSTALLATIONS NO LONGER FUNCTION PROPERLY DUE TO SEDIMENT OR DEBRIS. EVENTUAL SYSTEM CLEANING IS NOT AN EXCUSE TO NOT IMPLEMENT APPROPRIATE CONTROLS UPSTREAM. THE ENGINEER SHALL BE THE FINAL JUDGE REGARDING WHETHER THE PIPING SYSTEM REQUIRES CLEANING. THE CONTRACTOR CAN MINIMIZE THE NECESSITY OF EXTENSIVE SILT AND SEDIMENT ACCUMULATION REMOVALS BY EFFECTIVE IMPLEMENTATION OF THE SWPPP.
- DIVERSION SWALES AND OTHER EROSION CONTROL 4. ALL DISTURBED AREAS WILL BE FERTILIZED, SEEDED AND MULCHED ACCORDING TO LANDSCAPE RESTORATION SPECIFICATIONS TO MAINTAIN VIGOROUS, DENSE VEGETATION. REPAIR ANY ERODED SLOPES, REAPPLY TOPSOIL, RESEED AND STABILIZE REPAIR AREA AS REQUIRED FOR PERMANENT OR TEMPORARY MEANS. REPAIR SOIL AREAS DAMAGED BY EROSION OR CONSTRUCTION EQUIPMENT.
- PRACTICES (BMP'S). DOCUMENT WEEKLY INSPECTIONS IN 5. IMMEDIATELY REPAIR ANY DAMAGE CAUSED BY CONSTRUCTION EQUIPMENT, MAINTENANCE OR OTHER ACTIVITY TO ANY EROSION CONTROL MEASURE. OR BEST MANAGEMENT PRACTICE OR DEVICE.
 - 6. THE PRIME CONTRACTOR(S) ARE RESPONSIBLE FOR THE PERFORMANCE AND COMPLIANCE OF THEIR SUB-CONTRACTOR'S ACTIVITIES RELATING TO THE SWPPP. THEY SHALL MAKE FREQUENT INSPECTIONS OF THEIR WORK AND COORDINATE APPROPRIATE INSTALLATION AND MAINTENANCE OF EROSION CONTROL AND WATER QUALITY DEVICES.
- EARTHWORK AND STABILIZATION WORK PERFORMED. 7. EMPLOY POLLUTION PREVENTION MEASURES TO CONTROL LITTER. CONSTRUCTION CHEMICALS, SEDIMENT AND CONSTRUCTION DEBRIS INCLUDING, BUT NOT LIMITED, TO THE FOLLOWING: SALVAGE AND REUSE OF MATERIALS, MINIMIZING PACKAGING WASTE, RECYCLING, PROPER DISPOSAL AT FREQUENT INTERVALS IN ACCORDANCE WITH PREVAILING LAWS, ONSITE INSTRUCTION REGARDING APPROPRIATE SEPARATION/HANDLING/RECYCLING, PERIODIC DEBRIS REMOVAL AT DRAINAGE STRUCTURES (GRATES AND SUMPS)/SEDIMENT TRAPS/ FOREBAY AND OTHER BMP'S, PROPER MAINTENANCE OF SEDIMENT/ EROSION CONTROL SYSTEMS, ROUTINE AND EVENT RELATED INSPECTIONS OF DRAINAGE AND BMP SYSTEMS PER PERMIT REQUIREMENTS, PROVIDE APPROPRIATE SANITARY FACILITIES FOR ONSITE PERSONNEL, PICK UP TRASH AND DEBRIS FREQUENTLY AND USE WATER MIST, CALCIUM CHLORIDE OR OTHER LEGAL MEANS TO LIMIT THE SPREAD OF DUST AND SOIL PARTICLES.

PROTECTION OF TREES:

PROTECT EXISTING TREES WHICH ARE TO REMAIN AND WHICH MAY BE INJURED, BRUISED. DEFACED, OR OTHERWISE DAMAGED BY CONSTRUCTION OPERATIONS, UTILIZING STANDARD TREE PROTECTION CRITERIA INCLUDING:

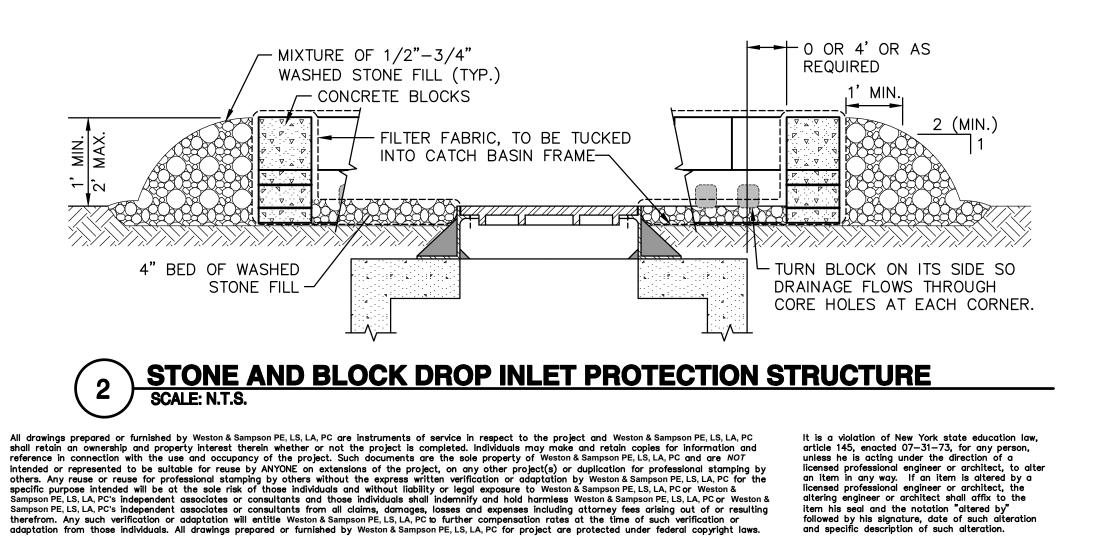
- A. INSTALLATION OF SAFETY ORANGE PLASTIC FENCING (MINIMUM 4' IN HEIGHT) AROUND INDIVIDUAL TREES DESIGNED FOR PROTECTION. FENCING SHALL BE INSTALLED AT THE OUTWARD LIMIT OF THE TREE'S DRIPLINE OR EXTENT OF CANOPY COVER.
- INSTALLATION OF SAFETY ORANGE PLASTIC FENCING (MINIMUM 4' IN HEIGHT AROUND GROUPS OF TREES DESIGNATED FOR PROTECTION.
- C. TREE AND/OR SHRUB BRANCHES IN THE WAY OF EQUIPMENT SHALL BE TRIMMED ACCORDING TO PROFESSIONAL HORTICULTURAL STANDARDS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR AND SUB-CONTRACTORS USE EQUIPMENT

TO DEMOLISH BRANCHES AS WORK PROCEEDS. REQUIRED FENCING SHALL BE INSTALLED PRIOR TO THE INITIATION OF LAND DISTURBING ACTIVITIES AND SHALL BE REMOVED AT THE CONCLUSION OF CONSTRUCTION. REMOVE DISPLACED ROCKS FROM UNCLEARED AREAS. BY APPROVED EXCAVATION, REMOVE TREES WITH 30 PERCENT OR MORE OF THEIR ROOT SYSTEMS DESTROYED. REMOVAL OF TREES AND THE PROCEDURE FOR REMOVAL REQUIRES APPROVAL OF THE CONTRACTING OFFICER. TREES DESIGNATED FOR REMOVAL SHALL BE REMOVED IN A MANNER THAT WILL NOT IMPACT ADJACENT TREES.

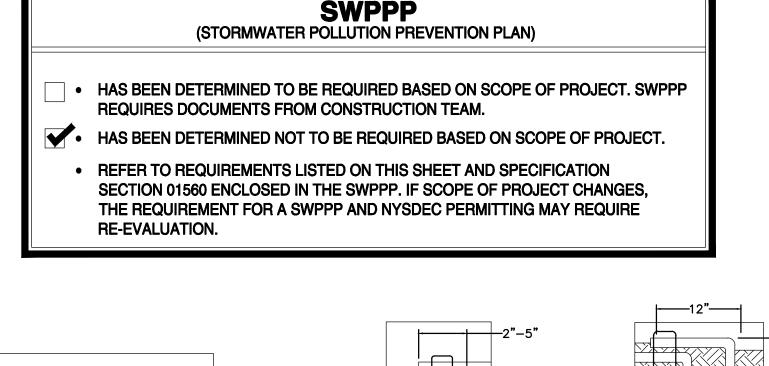
LANDSCAPE REPLACEMENT

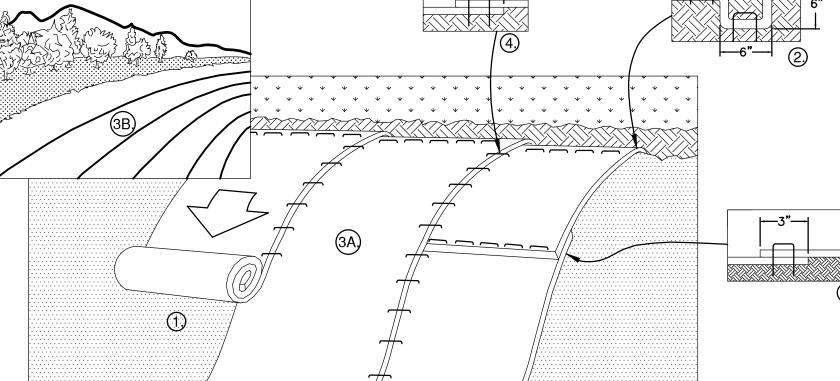
REMOVE TREES AND OTHER LANDSCAPE FEATURES SCARRED OR DAMAGED BY EQUIPMENT OPERATIONS, AND REPLACE WITH EQUIVALENT, UNDAMAGED TREES AND LANDSCAPE FEATURES. OBTAIN CONTRACTING OFFICER'S APPROVAL BEFORE REPLACEMENT. REPLACEMENT OF TREES SHALL OCCUR ON A ONE-TO-ONE BASIS. REGIONALLY NATIVE PLANTS AS SPECIFIED BY THE STATION "INTEGRATED NATURAL RESOURSES MANAGEMENT PLAN" (INRMP) SHALL BE USED AS REPLACEMENT LANDSCAPE FEATURES.











1. PREPARE SOIL BEFORE INSTALLING ROLLED EROSION CONTROL PRODUCTS (RECP's), INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED. NOTE: WHEN USING CELL-O-SEED DO NOT SEED PREPARED AREA. CELL-O-SEED MUST BE INSTALLED WITH PAPER SIDE DOWN.

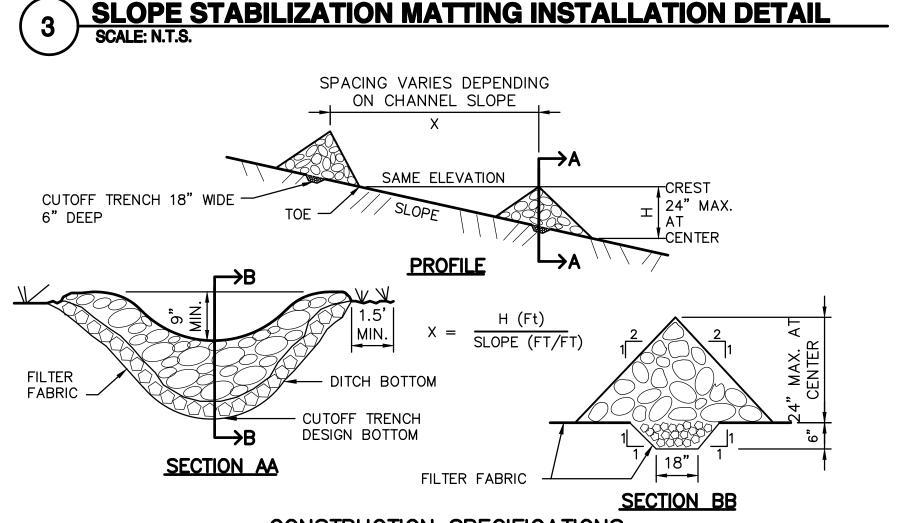
2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE RECP'S IN A 6" DEEP X 6" WIDE TRENCH WITH APPROXIMATELY 12" OF RECP'S EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE RECP'S WITH A ROW OF STAPLES/STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF RECP'S BACK OVER SEED AND COMPACTED SOIL. SECURE RECP'S OVER COMPACTED SOIL WITH A ROW OF STAPLES/STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE RECP's.

3. ROLL THE RECP'S (A.) DOWN OR (B.) HORIZONTALLY ACROSS THE SLOPE. RECP'S WILL UNROLL WITH APPROPRIATE SIDE AGAINST THE SOIL SURFACE. ALL RECP'S MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS SHOWN IN THE STAPLE PATTERN GUIDE. WHEN USING THE DOT SYSTEM , STAPLES/STAKES SHOULD BE PLACED THROUGH EACH OF THE COLORED DOTS CORRESPONDING TO THE APPROPRIATE STAPLE PATTERN.

4. THE EDGES OF PARALLEL RECP'S MUST BE STAPLED WITH APPROXIMATELY 2" - 5" OVERLAP DEPENDING ON RECP's TYPE.

5. CONSECUTIVE RECP'S SPLICED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATE 3" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART ACROSS ENTIRE ON RECP'S TYPE. NOTE: *IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 6" MAY BE

NECESSARY TO PROPERLY SECURE THE RECP's.



CONSTRUCTION SPECIFICATIONS

- 1. STONE WILL BE PLACED ON A FILTER FABRIC FOUNDATION TO THE LINES, GRADES AND LOCATIONS SHOWN IN THE PLAN.
- 2. SET SPACING OF CHECK DAMS TO ASSUME THAT THE ELEVATIONS OF THE CREST OF THE DOWNSTREAM DAM IS AT THE SAME ELEVATION OF THE TOE OF THE UPSTREAM DAM.
- 3. EXTEND THE STONE A MINIMUM OF 1.5 FEET BEYOND THE DITCH BANKS TO PREVENT CUTTING AROUND THE
- DAM
- 4. PROTECT THE CHANNEL DOWNSTREAM OF THE LOWEST CHECK DAM FROM SCOUR AND EROSION WITH STONE OR LINER AS APPROPRIATE. 5. ENSURE THAT CHANNEL APPURTENANCES SUCH AS CULVERT ENTRANCES BELOW CHECK DAMS ARE NOT
- SUBJECT TO DAMAGE OR BLOCKAGE FROM DISPLACED STONE. MAXIMUM DRAINAGE AREA = 2 ACRES. 6. STONE SHALL BE A WELL GRADED STONE MATRIX 2 TO 9 INCHES IN SIZE.
- (NYSDOT LIGHT STONE FILL MEETS THESE REQUIREMENTS).





١.	
2.	EMBED
3.	WHEN ⁻
	A MININ
4.	MAINTE
	PLAN"
5.	ENCIRC
6.	PREFAE



NOTES:

- 1. STABILIZATI STONE. OVE
- 2. ALL SURF PIPED BENE
- 3. WHEN EQU THE ENTRA RECEIVES REPRESEN
- 4. KEEP ROA ACCUMULA
- 5. REMOVE SIL NOT IMPAIR



2. 3.	SWALE A SWALE B C 1' D 4' 6' CROSS SECTION ALL TEMPORARY SWALES SHALL HAVE UNINTERRUPTED POSITIVE GRADE TO AN OUTLET. DIVERTED RUNOFF FROM A DISTURBED AREA SHALL BE CONVEYED TO A SEDIMENT TRAPPING DEVICE. DIVERTED RUNOFF FROM AN UNDISTURBED AREA SHALL OUTLET DIRECTLY INTO AN UNDISTURBED STABILIZED AREA AT NON-EROSIVE VELOCITY.		<u>ston(&)Sampson</u>	Weston & Sampson, PE, LS, LA, PC	uth Bedford Road, Suit ount Kisco, NY 10549	914.200.1077 800.SAMPSON www.westonandsampson.com
	ALL TREES, BRUSH, STUMPS, OBSTRUCTIONS, AND OTHER OBJECTIONABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF SO AS NOT TO INTERFERE WITH THE PROPER FUNCTIONING OF THE SWALE.		Ž			914.
	THE SWALE SHALL BE EXCAVATED OR SHAPED TO LINE, GRADE, AND CROSS SECTION AS REQUIRED TO MEET THE CRITERIA SPECIFIED HEREIN AND BE FREE OF BANK PROJECTIONS OR OTHER IRREGULARITIES WHICH WILL IMPEDE NORMAL FLOW.	[
7.	FILLS SHALL BE COMPACTED BY EARTH MOVING EQUIPMENT. ALL EARTH REMOVED AND NOT NEEDED ON CONSTRUCTION SHALL BE PLACED SO THAT IT WILL NOT INTERFERE WITH THE					
	FUNCTIONING OF THE SWALE. STABILIZATION SHALL BE AS PER THE CHART BELOW:					DATE
	MINIMUM FLOW CHANNEL STABILIZATION			tion		
	TYPE OF TREATMENT CHANNEL GRADE SWALE A (5ac. OR LESS) SWALE B (5ac10ac.)			Description		
	1 0.5 - 3.0% SEED AND STRAW MULCH SEED AND STRAW MULCH 2 3.1 - 5.0% SEED AND STRAW MULCH SEED USING JUTE OR EXCELSIOR			D	>	
	2 3.1 - 3.0% SEED AND STRAW MOLCH SEED USING JUTE OR EXCELSIOR 3 5.1 - 8.0% SEED WITH JUTE OR EXCELSIOR; SOD LINED RIP-RAP 4-8"	SFD F(.			
	48.1 - 20%LINED 4-8" RIP-RAPENGINEERED DESIGN	FIFAS			\bigcirc	Ŷ
	PERIODIC INSPECTION AND REQUIRED MAINTENANCE MUST BE PROVIDED AFTER EACH RAIN EVENT.	Ц Ц	! ┿─┿	<u> </u>	۲ ۲	ENGINEER
	FLOW CHANNEL STABILIZATION SHOWN ABOVE WILL REQUIRE SUBSEQUENT MAINTENANCE AND ATTENTION. SINCE RUNOFF PATTERNS, QUANTITY, QUALITY AND SEDIMENT CONTENT VARY WIDELY FROM SITE TO SITE, ACTUAL STABILIZATION MAY EXCEED THAT SHOWN IN THE CONTRACT DOCUMENTS. REMOVE SILT ACCUMULATIONS ROUTINELY AND DISPOSE OF		5	App.By		
	PROPERLY SUCH THAT WATER QUALITY IS NOT IMPAIRED. DO NOT INTRODUCE SILT INTO TOPSOIL/RESTORATION AREAS. REMOVE TEMPORARY SWALE WHEN SITE IS SUBSTANTIALLY OR COMPLETELY STABILIZED AND SWALE IS NO LONGER		++			PROFESSIONAL
	REQUIRED. PERFORM FINAL GRADING AND ESTABLISH VEGETATION AT ALL DISTURBED AREAS.	Š	;)	Ck.E	۵.	ROFES
5	SCALE: N.T.S.	ΔM		.By		
				Dr.		REGISTERED
	POSTS TO BE 2" x 2" X 3' HT. WOOD	672	; ;	Date		RE
	SPACING AS SITE CONDITIONS WARRANT.			Z		
	SILT FENCE FABRIC SELF SUPPORTING OR BACKED WITH WIRE MESH.			**	ICINEEF	
	FLOW FLOW FINISHED GRADE		N V N V O M			TAN
	6" MIN		OF NE			063680 DFESS
	= 16" MIN. ' [] = 16" MIN. '			, , ,		
					.I CEN	
	1. SILT FENCE FABRIC SHALL BE FIRMLY ATTACHED TO POSTS USING WIRE TIES OR STAPLES.					APP.BY JFB
	 EMBED FILTER CLOTH A MINIMUM OF 6" BELOW FINISHED GRADE. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVERLAPPED A MINIMUM OF SIX INCHES AND FOLDED. 				N	
	4. MAINTENANCE SHALL BE PERFORMED IN ACCORDANCE WITH THE "GENERAL MAINTENANCE PLAN" OR WHEN BULGES OF MATERIAL DEVELOP IN FENCES.				TAIL	CHK.BY CWS
	 5. ENCIRCLE ALL SOIL STOCKPILES, TEMPORARY OR PERMANENT. 6. PREFABRICATED UNITS SHALL BE GEOFAB, ENVIROFENCE OR APPROVED EQUIVALENT. 			z	Ы D	N
	SILT FENCE DETAIL			2 MAIN	ОГ	DSN.BY
	6 SCALE: N.T.S.			WATER	CONTROI	NZ NZ
	50' MINIMUM		QR V	POF	00	DR.B
			NEK	LINING	F	-0417
	6" CRUSHED STONE NYSDOT 703-4, TYPE 3 -				JEN	JOB NO. ENG21.
	CROWNED AND COMPACTED SUBGRADE		YORKTOWN,	MORTAR	SEDIMEN	5 Ш
	MIRAFI 500X OR APPROVED EQUIVALENT 10'R		OF YC	CEMENT		÷
	12' MIN. W/ TWO ACCESS PTS. 24' MIN. W/ ONE ACCESS PT.		\leq		AND	CONTRACT:
				3 AND		
NO				ANING	SION	NOTED
1.	. STABILIZATION FABRIC SHALL BE PLACED OVER THE ENTIRE ENTRANCE AREA PRIOR TO PLACING OF STONE. OVERLAP FABRIC PER MANUFACTURER'S SPECIFICATIONS.			CLE	ERO	SCALE: AS I
2	ALL SURFACE WATER FLOWING OR DIVERTED TOWARDS THE CONSTRUCTION ENTRANCE SHALL BE					.3 SC
	PIPED BENEATH THE ENTRANCE ROAD.				SOIL	
3	WHEN EQUIPMENT WASHING IS REQUIRED IT SHALL BE DONE ON A SEPARATE AREA ADJACENT TO THE ENTRANCE ROAD AND STABILIZED WITH STONE. EQUIPMENT WASHING WILL BE REQUIRED IF ROAD RECEIVES SIGNIFICANT SOILS OR DEBRIS ACCORDING TO JUDGMENT BY OWNER OR OWNER'S REPRESENTATIVE.					CADD NO.
4	. KEEP ROADS CLEAR OF STONES, MUD, AND OTHER CONSTRUCTION DEBRIS. CLEAN PAVEMENT AS ACCUMULATIONS WARRANT AND AS ORDERED BY ENGINEER.]
5	. REMOVE SILT ACCUMULATIONS ROUTINELY AND DISPOSE OF PROPERLY SUCH THAT WATER QUALITY IS				3	
	NOT IMPAIRED. DO NOT INTRODUCE SILT INTO DRAINAGE SYSTEM OR TOPSOIL/RESTORATION AREAS.)		N
	7 STABILIZED CONSTRUCTION ENTRANCE PAD DETAIL SCALE: N.T.S.	╏┠└				

COPYRIGHT 2022 WESTON & SAMPSON

SHEET 14 OF 18

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

- GENERAL MAINTENANCE AND PROTECTION OF TRAFFIC
- 1. THE MAINTENANCE AND PROTECTION PLANS SHOWN ARE STANDARDS FOR THE MOST COMMON SITUATIONS. ADDITIONAL PROTECTION SHALL BE PROVIDED WHERE SPECIAL CIRCUMSTANCES OCCUR.
- 2. ALL TRAFFIC CONTROL DEVICES USED FOR MAINTENANCE AND PROTECTION OF TRAFFIC SHALL CONFORM TO THE APPLICABLE SPECIFICATIONS AS SET FORTH IN THE NYSDOT M.U.T.C.D. (LATEST REVISIONS & AMENDMENTS) AND THE STANDARD SPECIFICATIONS (LATEST REVISIONS & AMENDMENTS).
- 3. THE CONTRACTOR MAY SUBMIT REVISIONS TO THIS PLAN FOR APPROVAL, BUT ANY CHANGE THAT ALTERS THE BASIC CONCEPTS OF THE PLAN MUST BE APPROVED BY THE REGIONAL DIRECTOR OR HIS DESIGNEE.
- 4. TRAFFIC SHALL BE MAINTAINED IN ACCORDANCE WITH ALL THE PROVISIONS OF STANDARD SPECIFICATIONS, ITEM 619.01 M - BASIC MAINTENANCE AND PROTECTION OF TRAFFIC, AND ITEM 619.02 M - CONSTRUCTION SIGNS, OR AS AMENDED ON THIS SHEET.
- . NO TEMPORARY LANE CLOSURES SHALL BE ALLOWED BEFORE SUNRISE OR AFTER SUNSET, OR AT OTHER TIMES WHEN VISIBILITY IS REDUCED TO LESS THAN 1000 FEET, UNLESS REQUIRED BY THE ENGINEER.
- 6. THE TRAVEL LANE WIDTH AVAILABLE SHALL BE 10 FEET MINIMUM AT ALL TIMES AND 10 FEET MINIMUM ON ALL OTHER ROADS, UNLESS OTHERWISE SHOWN IN THE PLANS.
- MOST WORK OPERATIONS ARE CONSIDERED TO BE LONG DURATION. OPERATIONS WHERE THE EQUIPMENT BEING USED PROGRESSES ALONG THE ROADWAY MORE OR LESS CONTINUOUSLY, AS IN SEEDING AND PAVEMENT MARKING OPERATIONS, CAN BE CONSIDERED MOVING WORK AREAS.
- 8. ALL SHADOW VEHICLES SHALL WEIGHT IN EXCESS OF 8,200 KG. SHADOW VEHICLES SHALL BE EQUIPPED WITH MOBILE CONSTRUCTION ZONE IMPACT ATTENUATOR, AS CALLED FOR UNDER STANDARD SPECIFICATIONS SUBSECTION 619-1.02. BASIC MAINTENANCE AND PROTECTION OF TRAFFIC.
- 9. VEHICLES BELONGING TO THE CONTRACTOR, OR THE CONTRACTOR'S EMPLOYEES, SHALL NOT BE PARKED ON THE PAVEMENT OR SHOULDER, OR WITHIN 20 FEET OF THE EDGE OF PAVEMENT ALONG OR ADJACENT TO TRAVEL LANES OPEN TO TRAVEL, WITHIN THE PROJECT LIMITS.
- 10. THE CONTRACTOR SHALL NOT PARK EQUIPMENT OR STORE MATERIAL OVERNIGHT WHERE IT IS DEEMED BY THE ENGINEER TO BE HAZARD TO TRAFFIC.
- 11. ALL VEHICLES THAT WILL BE MOVING IN AND OUT OF TRAFFIC AT WORK AREAS SHALL BE EQUIPPED WITH AN APPROVED AMBER ROTATING SAFETY LIGHT. THE LIGHTS SHALL BE MOUNTED SO AS TO BE EASILY SEEN BY APPROACHING TRAFFIC.
- 12. DRIVING AGAINST TRAFFIC AT ANY TIME, REGARDLESS OF WHETHER THE AREA HAS BEEN CLOSED TO TRAFFIC, SHALL NOT BE PERMITTED, EXCEPT FOR TRAFFIC CONE PICKUP, AND AS SPECIFICALLY PERMITTED BY THE ENGINEER.
- 13. ESCORT VEHICLES EQUIPPED WITH AN AMBER LIGHT OR AN OPERATING ARROW-BOARD WILL BE REQUIRED WHEN TRANSPORTING SLOW MOVING CONSTRUCTION EQUIPMENT ALONG ANY PORTION OF THE ROADWAY OPEN TO TRAFFIC.
- 14. WHEN REOPENING DRIVING LANES TO TRAFFIC, THE CONTRACTOR SHALL START BY REMOVING THE CONES AT THE FAR END OF THE LANE CLOSURE AND WORKING TOWARDS THE SIGNS AT THE BEGINNING OF THE LANE CLOSURE. THE SIGNS ARE NOT BE TAKEN DOWN UNTIL ALL TRAFFIC CONTROL DEVICES HAVE BEEN REMOVED.
- 15. PRIOR TO ANY REDUCTION IN EXISTING LANE WIDTH(S), THE CONTRACTOR SHALL PROVIDE THE ENGINEER TWENTY-ONE (21) DAYS NOTICE SO HE MAY INFORM THE REGIONAL PERMIT ENGINEER OF THE WIDTH REDUCTION(S) IN A TIMELY MANNER.
- 16. SEE SUBSECTION 619-3.01 IN THE STANDARD SPECIFICATIONS (LATEST REVISION) FOR REQUIRED TREATMENT OF PAVEMENT EDGE DROP-OFFS AND DELINEATION.
- 17. EXCAVATIONS THAT PRODUCE DROP-OFFS ON BOTH SIDES OF THE TRAVEL WAY AT THE SAME TIME SHALL NOT BE PERMITTED. SHOULDER AREAS SHOULD BE PREPARED TO RECEIVE THE SHOULDER PAVEMENT MATERIAL IMMEDIATELY AHEAD OF THE SHOULDER PAVING OPERATIONS TO MINIMIZE THE TIME A DROP-OFF EXISTS. "NO SHOULDER" (W4-13C) SIGNS SHALL BE ERECTED NO MORE THAN 400 METERS APART THROUGHOUT THE PROJECT WHERE A DROP-OFF EXISTS.
- 18. THE BOTTOM OF ALL TEMPORARY CONSTRUCTION SIGNS SHALL BE A MINIMUM OF 7 FEET ABOVE THE SURFACE.
- 19. THE CONTRACTOR SHALL PROVIDE FULL PATTERN PAVEMENT WARNINGS FOR TRAFFIC CONTROL IN CONFORMANCE WITH THE REQUIREMENTS OF STANDARD SPECIFICATIONS. ITEM 619.21 OX M - CONSTRUCTION ZONE PAVEMENT MARKINGS. IT MAY BE NECESSAR TO MARK, REMARK, ALTER AND/OR OBLITERATE PAVEMENT MARKINGS ON TEMPORARY AND PERMANENT PAVEMENT WHERE REQUIRED BY THE ENGINEER. THE MARKING SHALL BE REMOVED BY MEANS WHICH COMPLETELY REMOVE ALL TRACES OF STRIPES. THE ENGINEER SHALL CONTACT THE REGIONAL TRAFFIC ENGINEER ONE WEEK PRIOR TO TOF COURSE STRIPPING TO VERIFY PATTERN AND LIMITS. ALL TOP COURSE STRIPPING SHALL CONFORM TO THE PROJECT'S FINAL MARKINGS WITH RESPECT TO COLOR AND PATTERN.
- 20. ALL TEMPORARY STRIPING IS TO BE REMOVED WITHIN 48 HOURS AFTER THE INSTALLATION OF PERMANENT STRIPPING.
- 21. EXISTING GUIDE RAIL SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES NECESSITATE REMOVAL. NEW GUIDE RAIL SHALL BE INSTALLED WITHIN TWO WEEKS OF EXISTING RAIL REMOVAL UNLESS OTHERWISE APPROVED BY THE ENGINEER. TEMPORARY PROTECTION DEVICES OR DELINEATION SHALL BE INSTALLED AS REQUIRED BY THE ENGINEER. WHENEVER GUIDE RAILING IS REMOVED. THE CONTRACTOR SHALL INSTALL VERTICAL PANELS OR DRUMS IN ACCORDANCE WITH SECTION 292.3 AND 292.4 OF THE NYS MUTCD. THEY SHALL BE PLACED AT 30 FEET INTERVALS. THEY SHALL BE LEFT IN PLACE UNTIL THE NEW GUIDE RAIL IS INSTALLED OR THE SLOPE FLATTENING WORK IS COMPLETED.
- 22. SHORT DURATION ALTERNATING ONE-WAY TRAFFIC SHALL NOT BE ALLOWED UNLESS APPROVAL HAS BEEN GRANTED BY THE REGIONAL DIRECTOR OR HIS DESIGNEE. ALTERNATING ONE-WAY TRAFFIC MAY BE MAINTAINED ON SIDE ROADS DURING WORKING HOURS IN ACCORDANCE WITH DETAILS ON MPT NOTE 5 ABOVE, HOWEVER, THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE TWO UNOBSTRUCTED LANES FOR TWO-WAY TRAFFIC DURING NON-WORKING HOURS. FLAGGERS WILL BE REQUIRED TO USE RADIO OR SIMILAR CONTACT WHEN THEY ARE MAINTAINING ONE-WAY TRAFFIC WHEN ONE FLAGGER IS NOT VISIBLE TO THE OTHER OR IF, IN THE OPINION OF THE ENGINEER, THIS COMMUNICATION IS NECESSARY. THE COST OF ANY RADIO OR FIELD TELEPHONES USED SHALL BE INCLUDED IN THE BID PRICE.
- 23. THE COST OF PROVIDING AND MAINTAINING SAFE AND ADEQUATE INGRESS AND EGRESS TO AND FROM HOMES AND COMMERCIAL ESTABLISHMENTS AT ALL TIMES, TO THE SATISFACTION OF THE ENGINEER, SHALL BE BORNE BY THE CONTRACTOR. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR PROVIDING SAFE PEDESTRIAN ACCESS AT ALL TIMES.
- 24. IF NYSDOT NOTIFIES THE CONTRACTOR OR HIS SUPERINTENDENT OF ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO THE SATISFACTION OF NYSDOT BEFORE WORK IS RESUMED.
- 25. IN ORDER TO MAINTAIN EFFECTIVE TRAFFIC CONTROL, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE TRAFFIC CONTROL: MAKING SURE ALL SIGNS, CONES, FLASHERS, DRUMS, ETC. ARE IN PLACE AND IN GOOD CONDITION. THE SOLE JUDGE OF THE EFFECTIVENESS OF THE CONTRACTOR'S EFFORTS TOWARDS THE PROTECTION OF TRAFFIC AND PERSONNEL SHALL BE NYSDOT.
- 26. FLAGGERS SHALL BE LOCATED AT ALL ACTIVE WORK AREAS AND AT OTHER LOCATIONS WITHIN A WORK AREA WHERE RESTRICTED SIGHT DISTANCE IMPEDES THE FLOW OF TRAFFIC.
- 27. EXISTING TRAFFIC SIGNS SHALL BE COVERED AND UNCOVERED AS NECESSARY DURING CONSTRUCTION.
- 28. IF, IN THE JUDGEMENT OF THE OWNER, FLAGS ON SIGNS ARE NECESSARY DUE TO LIMITED SIGHT DISTANCE, THEY SHALL BE PROVIDED BY THE CONTRACTOR.

29. THE WORK AREA IS TO BE CLEANED DAILY AND LEFT IN AN ACCEPTABLE MANNER.

- 30. THE UNEVEN TRANSVERSE PAVEMENT SURFACES, (I.E., PAVEMENT REBATES, END OF PAVED OR MILLED SECTIONS) SHALL BE DUAL POSTED WITH "BUMP" (W4-1C) SIGNS SHALL BE PLACED ON A TYPE III BARRICADE AND ILLUMINATED WITH A FLASHING BEACON. ALL PAVEMENT TRANSITIONS SHALL HAVE NO STEEPER THAN 1 ON 4 LONGITUDINAL SLOPE AS MEASURED IN THE DIRECTION OF TRAVEL.
- 31. ALL REBATES SHALL BE PAVED WITHIN TWENTY-FOUR (24) HOURS.

- 33. THE CONTRACTOR SHOULD NOTE THAT BURIED UTILITIES EXIST WITHIN THE PROJECT LIMITS AND MAY ENCOUNTERED DURING CONSTRUCTION. EXTREME CARE SHOULD BE EXERCISED TO AVOID DAMAGE TO THESE FACILITIES. ANY DAMAGE SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPAIRS.
- 34. PEDESTRIAN AND BICYCLIST TRAFFIC SHALL BE MAINTAINED AND PROTECTED AT ALL TIMES IN ACCORDANCE WITH SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS. ALL PEDESTRIAN ACCESS SHALL CONFORM WITH THE CURRENT AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES.
- 35. NEW SIDEWALK CONSTRUCTION SHALL BE SCHEDULED SO THAT REMOVAL AND REPLACEMENT SHALL BE INITIATED AND COMPLETED ON ONE SIDE OF THE STREET PRIOR TO THE COMMENCEMENT OF WORK ON THE OPPOSITE SIDE. WORK IS TO BE SCHEDULED AND COMPLETED BETWEEN LOGICAL TERMINI; I.E., BETWEEN INTERSECTING STREETS IN ORDER TO ENSURE SAFE PEDESTRIAN CROSSING AND ACCESSIBILITY
- 36. "SIDEWALK CLOSED" SIGNS PLACED ON TYPE III BARRICADES (R8-3C) SHALL BE USED WHERE SIDEWALKS ARE CLOSED DUE TO CONSTRUCTION ACTIVITIES.
- 37. "SHARED ROADWAY" SIGNS (W5-6C AND W5-17 X MOD) SHALL BE INSTALLED, AS APPROPRIATE, WHERE TRAVEL LANES AND/OR SHOULDERS ARE BEING RECONSTRUCTED.
- 38. STOP/SLOW PADDLES USED AS SIGNALING DEVICES SHALL COMPLY WITH SECTION 293.2 OF THE NYSMUTCD, EXCEPT THAT THE MOUNTING HEIGHT SHALL BE 6 FEET MINIMUM.
- 39. THE CLEAR ROADSIDE AREA IS DEFINED AS THE AREA ADJACENT TO THE EDGE OF TRAVEL LANE WHICH MUST BE FREE OF OBSTACLES OR HAZARDS, THE CLEAR ROADSIDE AREA SHALL BE 20 FEET.
- 40. IN THE EVENT THAT THE CONTRACTOR ELECTS (WITH APPROVAL FROM THE ENGINEER), OR IS REQUIRED BY THE ENGINEER. TO SHUT DOWN OPERATIONS FOR THE WINTER SEASON PRIOR TO PROJECT COMPLETION. THE CONTRACTOR SHALL MAINTAIN TRAFFIC PATTERNS OF THE CONSTRUCTION PHASE IN EFFECT WHEN OPERATIONS CEASE. THE CONTRACTOR SHALL PROVIDE UNOBSTRUCTED TRAVEL LANES AT WIDTHS CORRESPONDING TO THOSE SHOWN ON THE CONSTRUCTION SEQUENCING PLANS, ON HARD ROADWAY SURFACES THROUGHOUT THE LENGTH OF THE PROJECT. FULL PAVEMENT STRIPING SHALL BE PROVIDED AND MAINTAINED AS SPECIFIED ON THE PLANS AND IN NOTE 20. DELINEATION DEVICES SHALL BE MOVED TO A DISTANCE OF 4 FEET MINIMUM FROM THE EDGE OF TRAVEL LANE TO PROVIDE FOR SNOW STORAGE, UNLESS OTHERWISE REQUIRED BY THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING SNOW WHICH ACCUMULATES AROUND DELINEATION DEVICES AND FOR KEEPING ALL IN PROPER WORKING CONDITION. ALL APPROACH CONSTRUCTION SIGNING SHALL REFLECT THE ACTUAL CONDITIONS AT ALL TIMES AS PER NYSMUTCD. IN ADDITION THE CONTRACTOR SHALL BE REQUIRED TO DO THE FOLLOWING:
 - A) PROVIDE ASPHALT WEDGES, WITH SUITABLE BOND BREAKERS, AROUND ALL DRAINAGE STRUCTURES WITHIN THE PAVEMENT AREA. ALL WATER VALVE BOXES AND MANHOLES SHALL BE SET AT THE PAVEMENT GRADE AT THE TIME OF SHUT DOWN AND SHALL BE ADJUSTED TO FINISHED GRADE THE FOLLOWING SEASON.
 - B) PROVIDE FULL PAVEMENT DELINEATION USING REFLECTORIZED PAVEMENT MARKING PAINT. CONFORMING TO SECTION 640 OF THE STANDARD SPECIFICATIONS.
 - ALL MISCELLANEOUS WORK REQUIRED TO PREPARE THE ROADWAY FOR WINTER SHUTDOWN SHALL BE INCLUDED IN THE BID PRICE.
- 41. THE CONTRACTOR IS RESPONSIBLE FOR SCHEDULING CULVERT AND DRAINAGE INSTALLATION TO PROVIDE ADEQUATE STORM WATER RUNOFF COLLECTION DURING ALL CONSTRUCTION PHASES. ANY TEMPORARY DRAINAGE CONNECTIONS, INLET ELEVATION ADJUSTMENTS, OR OTHER WORK NECESSARY OR ORDERED BY THE ENGINEER TO ENSURE RUNOFF COLLECTION SHALL BE INCLUDED IN THE BID PRICE.
- 42. THE CONTRACTOR SHALL LIMIT THE LENGTH OF THE WORK ZONE TO 1000 FEET OR AS SHOWN ON THE CONTRACT DRAWINGS. THE MINIMUM DISTANCE BETWEEN WORK ZONES SHALL BE 1000 FEET.
- 43. RUMBLE STRIPS, STANDARD SPECIFICATIONS ITEM 18619.6460 SHALL BE INSTALLED UPSTREAM OF ALL G11-1C SIGNS. EACH RUMBLE STRIP PATTERN SHALL CONSIST OF SIX (6) SEPARATE STRIPS PLACED ON 10 FEET CENTERS, WITH EACH STRIP TRAVERSING THE FULL WIDTH OF THE TRAVEL LANE. RUMBLE STRIPS FORMED BY APPLYING MULTIPLE LAYERS OF REMOVABLE PERFORMED PAVEMENT MARKING TAPE SHOULD NORMALLY BE USED. MILLED-IN, SAWCUT, OR RAISED ASPHALT STRIPS MAY BE USED ON PAVEMENT SURFACES THAT ARE SCHEDULED TO BE OVERLAID, REMOVED, OR REPLACED. THE CONTRACTOR SHALL REMOVE OR FILL IN THE RUMBLE STRIPS WHEN THEY ARE NO LONGER NECESSARY. RUMBLE STRIPS SHALL BE REMOVED OR FILLED IN BEFORE THE WINTER PLOWING SEASON AND BEFORE THE PLACEMENT OF AN OVERLAY.

HOLIDAY CONSTRUCTION RESTRICTIONS

- 1. THE CONTRACTOR SHALL CONSULT WITH THE REGIONAL DIRECTOR OR DESIGNEE FOR THE LATEST CALENDAR YEAR CONSTRUCTION RESTRICTIONS AND DURATION FOR ANY AND ALL HOLIDAYS THAT FALL WITHIN THE CONSTRUCTION PERIOD.
- II. CONSTRUCTION ACTIVITIES
- THAT AREA IN WHICH TRAFFIC IS RESTRICTED BECAUSE OF CONSTRUCTION ACTIVITIES OR THAT AREA WHICH INVOLVES A DROP-OFF WITHIN 30 FEET OF THE EDGE OF PAVEMEN
- 2. NO MATERIAL IS TO BE STORED WITHIN THE ROADSIDE AREA (DEFINED FOR THIS PROJECT AS THE AREA WITHIN 10 FEET OF THE EDGE OF PAVEMENT), EXCEPT THAT WHICH IS TO BE PLACED THAT DAY WITH THE APPROVAL OF THE ENGINEER.
- 3. CONSTRUCTION EQUIPMENT SHALL BE REMOVED FROM THE ROADSIDE AREA OF ALL HIGHWAY PAVEMENT DURING THE HOURS THAT THE CONTRACTOR IS NOT WORKING. THIS REQUIREMENT SHALL NOT BE LIMITED TO THE CONTRACT LIMITS.
- 4. CONTRACTOR VEHICLES NOT IN USE AND PRIVATE VEHICLES OWNED BY CONTRACTOR'S EMPLOYEES SHALL BE PARKED IN DESIGNATED PARKING AREA.
- 5. ALL OPERATIONS INCLUDING PAVING, ROLLING, GRADING, SUPPLYING OF MATERIAL AND THE TRAVEL SUPERVISORY PERSONNEL THROUGH THE WORK ZONE, AND THE ENTIRE LENGTH OF THE CONTRACT. WILL ALWAYS BE IN THE DIRECTION OF NORMAL TRAFFIC EXCEPT WHERE SPECIFICALLY ALLOWED OTHERWISE BY NYSDOT TO PERFORM A PARTICULAR OPERATION.
- 6. WHEN TRAFFIC IS TO BE MAINTAINED ON AN UNPAVED SURFACE DURING THE WORK DAY, A MINIMUM OF 12 INCHES OF SUBBASE MATERIAL, (ITEM 304.12) SHALL FIRST BE PLACED AND COMPACTED. THE CONTRACTOR SHALL USE CALCIUM CHLORIDE AS A MEANS OF DUST CONTROL. "PAVEMENT ENDS" SIGNS (W4-3C) SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 234 OF THE NYSMUTCD IN ADVANCE OF ALL SUCH AREAS AS REQUIRED BY THE ENGINEER. DURING NON—WORKING HOURS, THROUGH-TRAFFIC WILL BE MAINTAINED ON A PAVED SURFACE CONSISTING OF A MINIMUM OF 12 INCHES OF SUBBASE MATERIAL (ITEM 302.12) AND 2 INCHES OF TEMPORARY PAVEMENT (ITEM 403.1130001).
- 7. DUE TO THE HIGH DENSITY OF RESIDENTIAL AND COMMERCIAL PROPERTIES WITHIN THE CONTRACT LIMITS, THE FOLLOWING WORK HOUR RESTRICTIONS SHALL BE STRICTLY ENFORCED: NO WORK MAY BE PERFORMED BETWEEN THE HOURS OF 11:00 P.M. AND 7:00 A.M.

NO WORK MAY BE PERFORMED BETWEEN THE HOURS OF 11:00 P.M. AND 7:00 A.M. TRAFFIC SIGNALS: NONE

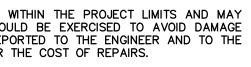
COORDINATION WITH UTILITIES

THE CONTRACTOR SHALL COOPERATE AND COORDINATE WITH ALL OTHER CONTRACTORS AND UTILITY COMPANIES OCCUPYING THE WORK SITE. THE FOLLOWING UTILITY COMPANIES MAY BE REQUIRED TO BE CONTACTED THROUGHOUT THE PROJECT CORRIDOR:

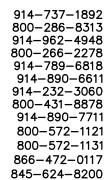
ALGONQUIN GAS & ELECTRIC AT&T CABLEVISION COMCAST CON EDISON NY TELEPHONE NYS DEPARTMENT OF PUBLIC WORKS TIME WARNER CABLE VERIZON NYSEG (GAS) NYSEG (ELECTRIC) OPTIMUM (TELEPHONE) OPTIMUM (CABLE)

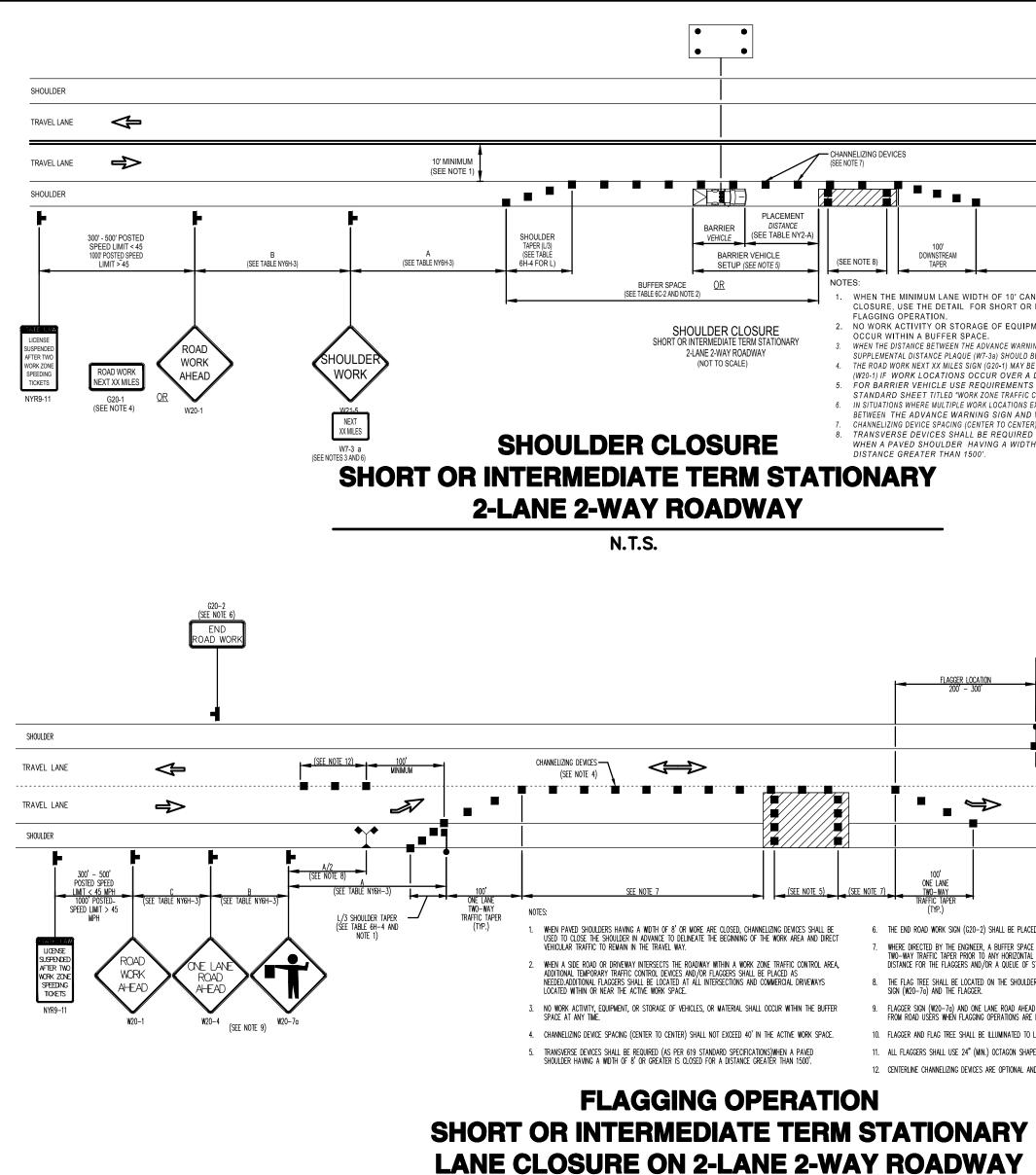
All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are NOT intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC for the specific purpose intended will be at the sole risk of those individuals and without liability or legal exposure to Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC to further compensation rates at the time of such verification or adaptation from those individuals. All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

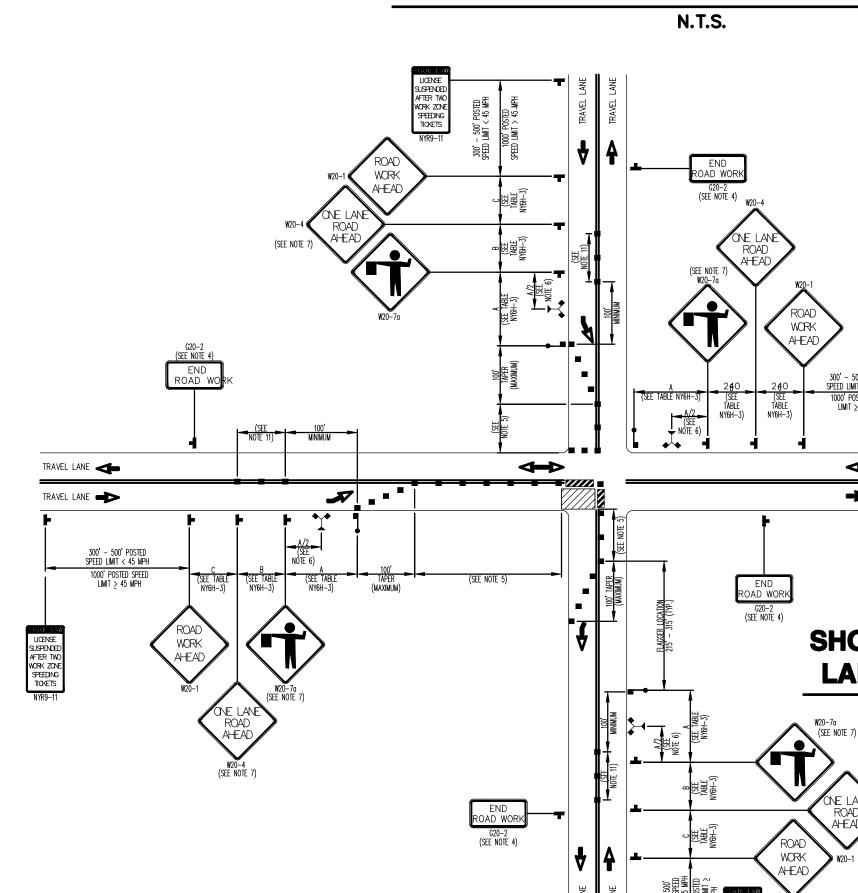
It is a violation of New York state education law article 145, enacted 07-31-73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a censed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.



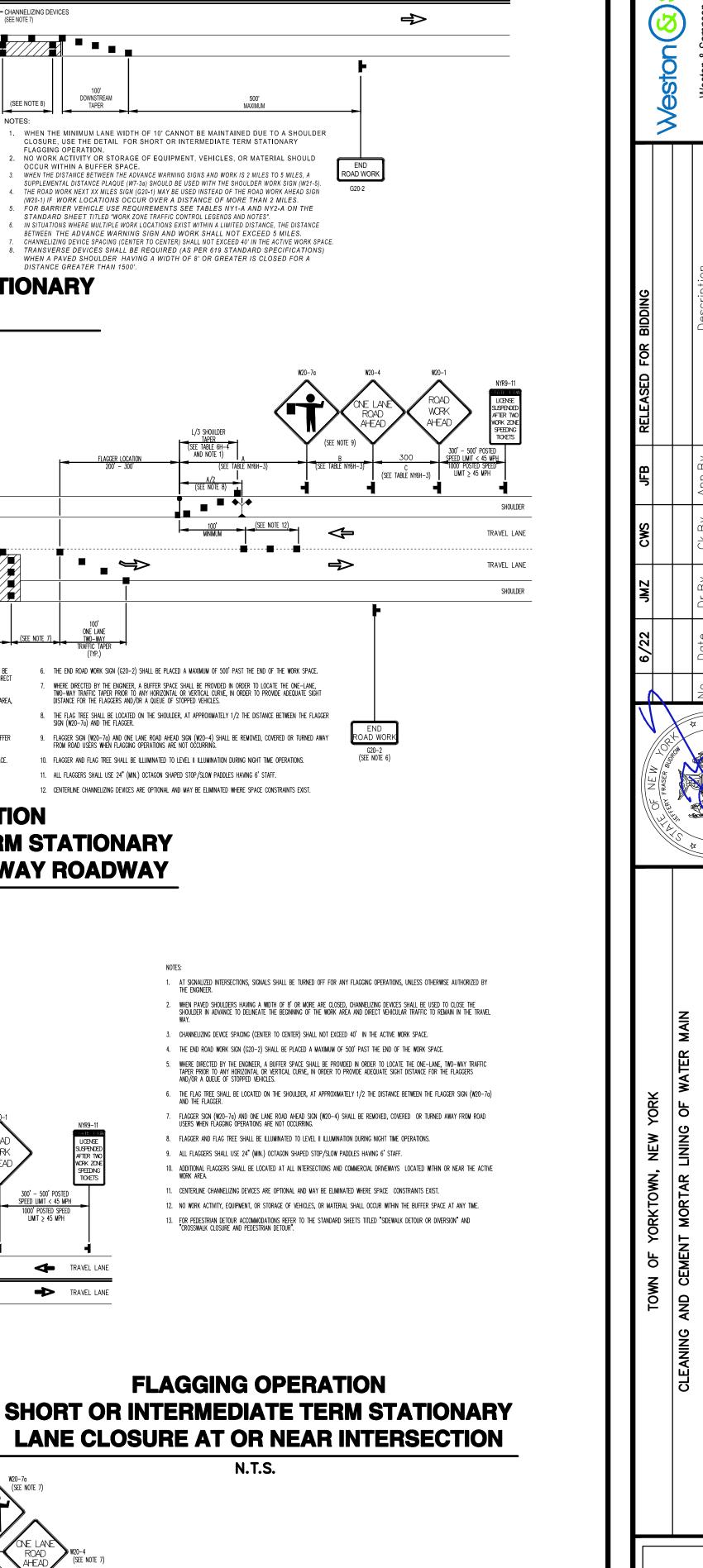
WORK ZONES ON OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP. A WORK ZONE IS DEFINED AS











ပ်ပ

Ο

 \mathbf{O}

 \cap

لىا

AIN.

()

R

SHEET

′15 o**F**18

TABLE NY1-A BARRIER VEHICLE USE REQUIREMENTS (LONG TERM, INTERMEDIATE TERM, AND SHORT TERM STATIONARY CLOSURES)						
			USE REQUI	REMENTS 4,5		
CLOSURE TYPE	EXPOSURE CONDITION 1	FREEWAY	NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIM			
			w 45 MPH	35-40 MPH	I 30 MPH	
	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED ³	REQUIRED ³	REQUIRED ³	OPTIONAL ²	
LANE CLOSURE	NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED	REQUIRED ³	REQUIRED ³	OPTIONAL ²	OPTIONAL ²	
	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED ³	REQUIRED ³	OPTIONAL ²	OPTIONAL ²	
SHOULDER CLOSURE	NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED	REQUIRED ³	OPTIONAL ²	OPTIONAL ²	OPTIONAL ²	

TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS										
SPEED LIMIT (MPH)	(S)	TAP (FT.	ER LENGTH)	1 (L)						
(40 MPH) OF	R LESS	L =	WS ² /60		L = TAPER LENGTH W = WIDTH OF OFFSET S = PRECONSTRUCTION		W = WIDTH OF OFFSET (FT.)		CLENGTH OF OFFSET (FT.) ONSTRUCTION POSTED SPEED LIMIT (MPH)	
(45 MPH) OF	R MORE	L =	WS							
STANDARD TAPER LENGTHS										
LATERAL SHIFT OF TRAFFIC										
FLOW PATH	(25 MPH)	(30 MPH)	(35 MPH)	(40 MPH)	(45 MPH)	(50 MPH)	(55 MPH)	(60 MPH)	(65 MPH)	(70 MPH
4	45	60	85	110	180	200	220	240	260	280
5	55	75	105	135	225	250	275	300	325	350
6	65	90	125	160	270	300	330	360	390	420
7	75	105	145	190	315	350	385	420	455	490
8	85	120	165	215	360	400	440	480	520	560
9	95	135	185	240	405	450	495	540	585	630
10	105	150	205	270	450	500	550	600	650	700
11	115	165	225	295	495	550	605	660	715	770
12	125	180	245	320	540	600	660	720	780	840

- THE EXPOSURE CONDITIONS DESCRIBED IN TABLE NY1-A ASSUMES THERE IS NO POSITIVE PROTECTION (TEMPORARY TRAFFIC BARRIER) PRESENT. WHERE WORKERS OR HAZARDS ARE PROTECTED BY A TEMPORARY TRAFFIC BARRIER, BARRIER VEHICLES ARE NOT REQUIRED.
- 2. WHERE THE REQUIREMENT IS "OPTIONAL", EITHER A BARRIER VEHICLE OR THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
- 3. REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE BARRIER VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE BARRIER VEHICLE SHALL BE REPOSITIONED ACCORDINGLY. BARRIER VEHICLES PROTECTING NON-TRANSVERSABLE HAZARDS SHALL REMAIN IN PLACE DURING BOTH WORKING AND NON-WORKING HOURS UNTIL THE HAZARD NO LONGER EXISTS. EXCEPTIONS TO THESE REQUIREMENTS MAY BE MADE, AS APPROVED BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE WHERE BARRIER VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC.
- 4. BARRIER VEHICLES ARE NOT REQUIRED FOR MILLING AND/OR PAVING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
- 5. BARRIER VEHICLES ARE NOT REQUIRED FOR FLAGGING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE6C-2) SHALL BE PROVIDED.

TABLE NY1-B SHADOW VEHICLE USE REQUIREMENTS (MOBILE CLOSURES)							
	REMENTS						
CLOSURE TYPE EXPOSURE CONDITION		FREEWAY	NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIN				
		FREEWAT	w 45 MPH	35-40 MPH	I 30 MPH		
LANE CLOSURE	WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED ^{2,4}	REQUIRED ^{2,4}	REQUIRED ^{2,4}	REQUIRED ^{2,4}		
SHOULDER CLOSURE	SHOULDER CLOSURE WHEN ANY WORKER, VEHICLE, OR OTHER HAZARD IS EXPOSED TO TRAFFIC		REQUIRED ^{2,4}	REQUIRED ^{2,4}	REQUIRED ^{2,4}		

- 1. A MOBILE CLOSURE SHALL BE USED FOR ANY WORK ACTIVITY THAT MOVES CONTINUOUSLY OR INTERMITTENTLY ALONG THE TRAVELED WAY OR SHOULDER SLOWER THAN THE PREVAILING SPEED OF TRAFFIC. CHANNELIZING DEVICES ARE NOT USED FOR MOBILE CLOSURES.
- 2. SHADOW VEHICLES SHALL BE EQUIPPED WITH AN APPROVED REAR MOUNTED ATTENUATOR (TRUCK MOUNTED OR TRAILER MOUNTED) FOR THE FOLLOWING MOBILE CLOSURES: LANE CLOSURES ON FREEWAYS, LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 35 MPH OR MORE, SHOULDER CLOSURES ON FREEWAYS, AND SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE.
- 3. FOR MOBILE LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 30 MPH OR LESS AND MOBILE SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 40 MPH OR LESS, SHADOW VEHICLES ARE NOT REQUIRED TO BE EQUIPPED WITH A REAR MOUNTED ATTENUATOR.
- 4. A SHADOW VEHICLE IS USED TO PROTECT EXPOSED WORKERS (ON FOOT OR IN A VEHICLE) AND SHALL BE REQUIRED FOR ALL MOBILE CLOSURES. SHADOW VEHICLE REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE SHADOW VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED PAVED SHOULDER 8' OR GREATER IN WIDTH. ADDITIONAL SHADOW VEHICLES MAY BE REQUIRED TO PROMOTE THE SAFE OPERATION OF TRAFFIC AND THE INCREASED PROTECTION OF EXPOSED WORKERS, AS DIRECTED BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE.

All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are NOT reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are NOT intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants and those individuals shall indemnify and hold harmless Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

It is a violation of New York state education law, article 145, enacted 07—31—73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a licensed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.

TABLE 6C-3 TAPER LENGTH FOR TEMPORARY TRAFFIC CONTROL ZONES

TYPE OF TAPER	TAPER LENGTH (L)
MERGING TAPER	L
SHIFTING TAPER	L/2
SHOULDER TAPER	L/3
ONE-LANE, TWO-WAY TRAFFIC TAPER	100 FT. MAXIMUM
DOWNSTREAM TAPER	100 FT. PER LANE

TABLE 6C-2 LONGITUDINAL BUFFER SPACE					
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE				
25	155 FT.				
30	200 FT.				
35	250 FT.				
40	305 FT.				
45	360 FT.				
50	425 FT.				
55	495 FT.				
60	570 FT.				
65	645 FT.				

TABLE NY2-A PLACEMENT DISTANCE FOR BARRIER VEHICLES

PRECONSTRUCTION POSTED	PLACEMENT DISTANCE (FT.)						
	BARRIER VEHICLES*						
SPEED LIMIT	(18000 LBS.)		(24000 LBS.)				
(MPH)	MINIMUM	MAXIMUM	MINIMUM	MAXIMUM			
> 55	100 FT.	200 FT.	100 FT.	200 FT.			
45 - 55	100 FT.	200 FT.	85 FT.	165 FT.			
< 45	85 FT.	165 FT.	50 FT.	100 FT.			

* AS DEFINED IN NYSDOT STANDARD SPECIFICATION 619:

PR

PO

SPI

BARRIER VEHICLE – VEHICLE USED FOR STATIONARY SHOULDER CLOSURES, LANE CLOSURES, AND OTHER STATIONARY WORK ZONES. MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

TABLE NY2-B PLACEMENT DISTANCE FOR SHADOW VEHICLES							
ECONSTRUCTION	PLACEMENT DISTANCE (FT.)						
STED	SHADOW VEHICLES**						
EED LIMIT	(18000	_BS.)	(24000 LBS.)				
PH)	MINIMUM	MAXIMUM	MINIMUM	MAXIMUM			
> 55	230 FT.	330 FT.	180 FT.	280 FT.			
45 - 55	180 FT.	280 FT.	150 FT.	250 FT.			
< 45	100 FT.	200 FT.	100 FT.	200 FT.			

* AS DEFINED IN NYSDOT STANDARD SPECIFICATION 619:

SHADOW VEHICLE - VEHICLE USED FOR MOBILE OR SHORT DURATION WORK OPERATIONS.

MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

	POSTED SPEEL			
TYPE OF POSITIVE BARRIER	30 MPH	40 MPH	50 MPH	
TEMPORARY CONCRETE BARRIER	8:1	11:1	14:1	
BOX BEAM OR HEAVY POST CORRUGATED BEAM	7:1	9:1	11:1	

TABLE NY6H-3 ADVANCE WARNING SIGN SPACING

TABLE 619-4 FLARE RATES FOR POSITIVE BARRIER

ADVANCE WARINING SIGN SPACING							
	DISTANCE	DISTANCE BETWEEN SIGNS SIGN LEGEN					
ROAD TYPE	A (FT.)	B (FT.)	C (FT.)	xx	YY		
URBAN (30 MPH*)	100	100	100	AHEAD	AHEAD		
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD		
URBAN (w 45 MPH*)	350	350	350	1000 FT.	AHEAD		
RURAL	500	500	500	1500 FT.	1000 FT.		
EXPRESSWAY / FREEWAY	1000	1500	2640	1 MILE	? MILE		

* PRECONSTRUCTION POSTED SPEED LIMIT

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA) SIDEWALKS, BICYCLE USAGE, CURBING, CLOSED DRAINAGE SYSTEMS, DRIVEWAY DENSITIES GREATER THAN 24 DRIVEWAYS PER MILE, MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS PER MILE OR GREATER, MAJOR COMMERCIAL DRIVEWAYS, NUMEROUS RIGHT OF WAY CONSTRAINTS, HIGH DENSITY OF CROSS STREETS, 85TH PERCENTILE SPEEDS OF 45 MPH OR LESS.

RURAL: ANY AREA NOT EXHIBITING MORE THAN ONE OF THE ABOVE CHARACTERISTICS.

EXPRESSWAY: DIVIDED HIGHWAYS FOR TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS.

FREEWAYS/INTERSTATE: LOCAL OR INTER REGIONAL HIGH-SPEED, DIVIDED, HIGH-VOLUME FACILITIES WITH FULL OR PARTIAL CONTROL OF ACCESS.

WORK	DURATION	DEFINITIONS

LONG-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MOR THAN 3 CONSECUTIVE DAYS.

INTERMEDIATE-TERM STATIONARY IS WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.

SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.

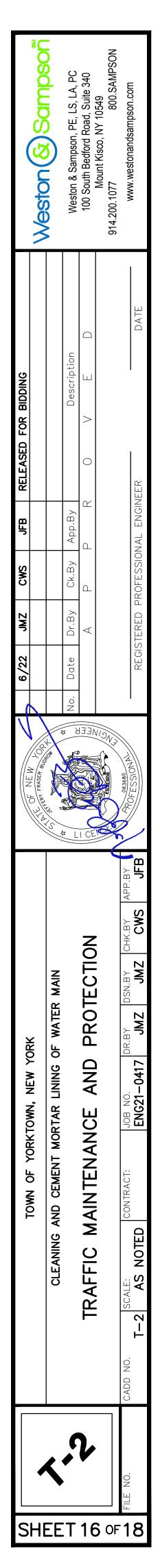
SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR. MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

65
MPH
20:1
15:1
•

2			
Ŷ			
Ε			

RE	
TION	

WOF	RK ZONE TRAFFIC CONTROL LEGEND
SYMBOL	DESCRIPTION
••••••	ARROW PANEL
• • • •	ARROW PANEL, CAUTION MODE
466	ARROW PANEL TRAILER OR SUPPORT
⊢-I	CHANGEABLE MESSAGE SIGN (PVMS)
	CHANNELIZING DEVICE
	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE
	LUMINAIRE
/////	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
┣	SIGN, TEMPORARY
	TEMPORARY BARRIER
	TEMPORARY BARRIER WITH WARNING LIGHTS
0—	TRAFFIC OR PEDESTRIAN SIGNAL
	TYPE III BARRICADE
<u>0</u>	WARNING LIGHTS
	WORK SPACE
	WORK VEHICLE
	WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR
	1



WORK ZONE TRAFFIC CONTROL SIGN TABLE						
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWA`	
EXIT	E5-1	С		72"X60"	72"X60"	
ROAD WORK NEXT X MILES	G20-1	A	36"X18"	48"X24"	48"X24"	
END ROAD WORK	G20-2	A	36"X18"	48"X24"	48"X24"	
PILOT CAR FOLLOW ME	G20-4	A	36"X18"			
WORK ZONE	G20-5aP	A	24"X18"	36"X24"	36"X24"	
	M1-1	G	1 OR 2 DIGITS 24"X24"	36"X36"	36"X36"	
	M1-1t	G	3 DIGITS 30"X24"	45"X36"	45 " X36"	
\times \times	M1-4	В	1 OR 2 DIGITS 24"X24"	36"X36"	36"X36"	
$\langle \times \rangle$	M1-4t	В	3 DIGITS 30"X24"	45"X36"	45"X36"	
NORTH	M3-1					
	M3-2	SEE	24"X12"	36"X18"	36"X18"	
WEST	M3-3 M3-4	NOTE 3				
DETOUR	M4-8	A	24"X12"	36"X18"	36"X18"	
END DETOUR	M4-8a	A	24"X18"	24"X18"	24"X18"	
DETOUR ETOUR DETOUR	M4-9 M4-9L M4-9R	A	30"X24"	48"X36"	48"X36"	
	M4-9a	A	30"X24"	30"X24"		
DETOUR DETOUR	M4-9b	A	30"X24"	30"X24"		
DETOUR DETOUR DETOUR	M4-9c	A	30"X24"	30"X24"		
DETOUR	M4-10L		48"X18"	48"X18"	48"X18"	
DETOUR	M4-10R					
	M5-1	SEE NOTE 3	21"X15"	30"X21"	30"X21"	
<u> </u>	M5-2	SEE NOTE 3	21"X15"	30"X21"	30"X21"	
← →	M6-1					
	M6-2	SEE	21"X15"	30"X21"	30"X21"	
	M6-3	NOTE 3				
↔	M6-4					
\times \times	NYM3-1	В	24"X24"	36"X36"	36"X36"	
	NYM3-2	В	30"X24"	45"X36"	45"X36"	
	NYM3-3	В	30"X24"	45"X36"	45"X36"	

WORK ZONE TRAFFIC CONTROL SIGN TABLE							
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY		
STATE LAW LICENSE SUSPENDED AFTER TWO WORK ZONE SPEEDING TICKETS	NYR9-11	В	24"X42"	48"X84"	48"X84"		
STATE LAW FINES XUBLED FOF SPEEDING IN WORK ZONES	NYR9-12	В	24"X36"	36"X54"	48"X72"		
RUMBLE STRIPS	NYW4-17	A	36"X36"	48"X48"	48"X48"		
WET PANT	NYW8-30	А	48"X24"	48"X24"	48"X24"		
STAY IN LANE	NYW8-31	A	48"X24"	48"X24"	48"X24"		
DO NOT PASS	NYW8-32	А	48"X24"	48"X24"	48"X24"		
LANE CLOSED	NYW8-33	A	48"X24"	48"X24"	48"X24"		
STOP	R1-1	D	36"X36"	36"X36"	48"X48"		
YIEL	R1-2	E	36"X36"X36"	48"X48"X48"	60"X60"X60"		
SPEED LIMT XX	R2-1	В	24"X30" OR 30"X36" (SEE NOTE 5)	36"X48"	36"X48"		
END HG-ER FINES ZONE	R2-11	В	24"X30"	36"X48"	36"X48"		
END WORK ZONE SPEED LIMT	R2-12	В	24"X36"	36"X54"	36"X54"		
DO NOT PASS	R4-1	В	24"X30"	36"X48"	36"X48"		
	R4-7	B	24"X30"	36"X48"	36"X48"		
1	R4-7c NARROW	В	18"X30"				
	R4-8	В	24 " X30"	36"X48"	36"X48"		
	R4-8c NARROW	В	18"X30"				
STAY IN LANE	R4-9	В	24"X30"	36"X48"	36"X48"		
DO NOT ENTER	R5-1	E	36"X36"	36"X36"	48"X48"		
PEDESTRIAN CROSSWALK	R9-8	В	36"X18"	36"X18"			
SDEWALK CLOSED	R9-9	В	24"X12"	24"X12"			
SDEWALK OLOSED USE OTHER SDE SDEWALK OLOSED USE OTHER SDE	R9-10L R9-10R	В	24"X12"	24"X12"			
SDEWNIK (LOSED AFEAD CROSS HERE SDEWNIK (LOSED AFEAD CROSS HERE	R9–11L R9–11R	В	24"X18"	24"X18"			
SDEWALK QLOSED CROSS HERE SDEWALK QLOSED CROSS HERE	R9-11aL R9-11aR	В	24"X12"	24"X12"			
STOP HERE ON RED	R10-6	В	24"X36"	24"X36"			
ROAD CLOSED	R11-2	В	48"X30"	48"X30"	48"X30"		

All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are NOT intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC for the specific purpose intended will be at the sole risk of those individuals and without liability or legal exposure to Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants and those individuals shall indemnify and hold harmless Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

It is a violation of New York state education law, article 145, enacted 07-31-73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a licensed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.

	WORK Z	ZONE TRAF	FIC CONTROL SIGN	TABLE	
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	
ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY	R11–3a	В	60"X30"	60"X30"	
	W1-4L W1-4R	A	36"X36"	48"X48"	
	W1-4bL W1-4bR	A	36"X36"	48"X48"	
**** ****	W1-4cL W1-4cR	A	36"X36"	48"X48"	
	W1-6L	A		oo") (70"	
	W1-6R	A	48"X24"	60"X30"	
K	W1-8L	A (NO BORDER)			
	W1-8R	A (NO BORDER)	18"X24"	30 " X36"	
	W3-1	A ⁴	36"X36"	48"X48"	
	W3-2	A4	36"X36"	48"X48"	
	W3-3	A ⁴	36"X36"	48"X48"	
BE PREPARED TO STOP	W3-4	A	36"X36"	48"X48"	
	W3-5	A ⁴	36"X36"	48"X48"	
	W4-1L W4-1R	A	36"X36"	48"X48"	
	W4-2L W4-2R	A	36"X36"	48"X48"	

FREEWAY λΥ _____ 48"X48" 48"X48" 48"X48" 60"X30" 30"X36" 48"X48" 48"X48" 48"X48" 48"X48" 48"X48" 48"X48" 48"X48"

COLOR CODE LEGEND CODE DESCRIPTION BLACK LEGEND AND BORDER А ON AN ORANGE BACKGROUND BLACK LEGEND AND BORDER R ON A WHITE BACKGROUND WHITE LEGEND AND BORDER С ON A GREEN BACKGROUND WHITE LEGEND AND BORDER D ON A RED BACKGROUND RED LEGEND AND BORDER F ON A WHITE BACKGROUND BLACK LEGEND AND BORDER F ON A FLOURESCENT YELLOW GREEN BACKGROUND WHITE LEGEND AND BORDER G ON A BLUE AND RED BACKGROUND

NOTES:

ROADWAY DEFINITIONS:

CONVENTIONAL ROAD – A STREET OR HIGHWAY OTHER THAN

EXPRESSWAY – A DIVIDED HIGHWAY WITH PARTIAL CONTROL

FREEWAY – A DIVIDED HIGHWAY WITH FULL CONTROL OF ACCESS.

OF ACCESS.

A FREEWAY, OR EXPRESSWAY.

1. DIMENSIONS ARE SHOWN AS WIDTH X HEIGHT.

2. FOR SIGNAGE NOT SHOWN ON THESE TABLES REFER TO THE M.U.T.C.D.

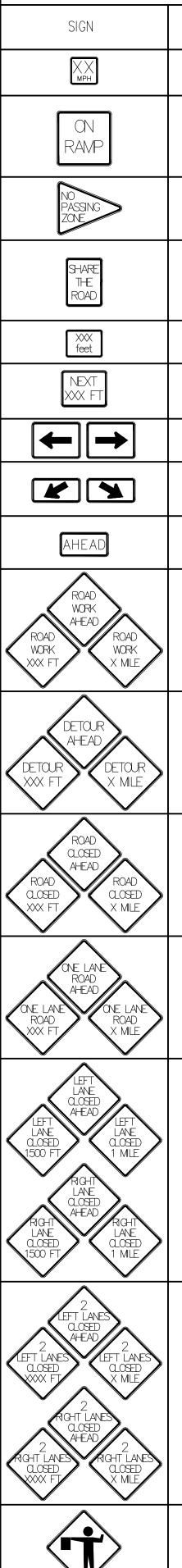
3. COLORS FOR DIRECTION PLAQUES, ADVANCE TURN ARROWS, AND DIRECTIONAL ARROWS SHALL MATCH THE ROUTE OR INTERSTATE SIGN THAT THEY SUPPLEMENT AS PER THE M.U.T.C.D.

4. MULTICOLORED SYMBOL IMPOSED ON SIGN WITH BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND.

5. FOR R2-1 SIGN LARGER DIMENSIONS SHALL BE USED WHEN SIGN FACES MULTIPLE LANES ON A CONVENTIONAL ROAD.

Weston & Sampson		VVeston & Sampson, PE, LS, LA, PC 100 South Bedford Road, Suite 340 Maint Kisco, NV 10540	914.200.1077 800.SAMPSON www.westonandsampson.com
6/22 JMZ CWS JFB RELEASED FOR BIDDING		A P P R O V E D	REGISTERED PROFESSIONAL ENGINEER
TOWN OF YORKTOWN, NEW YORK	CLEANING AND CEMENT MORTAR LINING OF WATER MAIN	TRAFFIC MAINTENANCE AND PROTECTION	CADD NO. T-3 AS NOTED CONTRACT: JOB NO. JNZ CWS APP.BY APP
	</td <td>3</td> <td>FILE NO.</td>	3	FILE NO.

WORK ZONE TRAFFIC CONTROL SIGN TABLE						
SIGN	DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY	
ROAD	W5-1	A	36"X36"	48"X48"	48"X48"	
RAMP NARROWS	W5-4	A	36"X36"	48"X48"	48"X48"	
	W6-3	A	36"X36"	48"X48"	48"X48"	
NEXT X MLES	W7-3aP	A	24"X18"	36"X30"	36"X30"	
BUMP	W8-1	A	36"X36"	48"X48"	48"X48"	
PAVEMENT ENDS	W8-3	A	36"X36"	48"X48"	48"X48"	
LOOSE GRAVEL	W8-7	A	36"X36"	48"X48"	48"X48"	
ROUGH ROAD	W8-8	A	36"X36"	48"X48"	48"X48"	
LOW SHOULDER	W8-9	A	36"X36"	48"X48"	48"X48"	
NO CENTER UNE	W8-12	A	36"X36"			
FALLEN ROCKS	W8-14	A	36"X36"	48"X48"	48"X48"	
CROOVED PAVEMENT	W8-15	A	36"X36"	48"X48"	48"X48"	
	W8-17	A	36"X36"	48"X48"	48"X48"	
SHOUDER DROP-OFF	W8-17p	A	24"X18"	30"X24"	30"X24"	
SHOLDER	W8-23	A	36"X36"	48"X48"	48"X48"	
STEEL PLATE ON PAVEMENT	W8-24	A	36"X36"	48"X48"	48"X48"	
CENTER LANE CLOSED AHEAD	W9-3	A	36"X36"	48"X48"	48"X48"	
	W11-1L W11-1R	A OR F	36"X36"	36"X36"		
$\dot{\mathbf{x}}$	W11-2L W11-2R	F	36"X36"	36"X36"		
	W11-15L W11-15R	F	36"X36"	36"X36"		



All drawings prepared or furnished by Weston & Sampson PE, LS, LA, PC are instruments of service in respect to the project and Weston & Sampson PE, LS, LA, PC shall retain an ownership and property interest therein whether or not the project is completed. Individuals may make and retain copies for information and reference in connection with the use and occupancy of the project. Such documents are the sole property of Weston & Sampson PE, LS, LA, PC and are NOT intended or represented to be suitable for reuse by ANYONE on extensions of the project, on any other project(s) or duplication for professional stamping by others. Any reuse or reuse for professional stamping by others without the express written verification or adaptation by Weston & Sampson PE, LS, LA, PC for the specific purpose intended will be at the sole risk of those individuals and without liability or legal exposure to Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants and those individuals shall indemnify and hold harmless Weston & Sampson PE, LS, LA, PC or Weston & Sampson PE, LS, LA, PC's independent associates or consultants from all claims, damages, losses and expenses including attorney fees arising out of or resulting therefrom. Any such verification or adaptation will entitle Weston & Sampson PE, LS, LA, PC for project are protected under federal copyright laws.

It is a violation of New York state education law, article 145, enacted 07-31-73, for any person, unless he is acting under the direction of a licensed professional engineer or architect, to alter an item in any way. If an item is altered by a licensed professional engineer or architect, the altering engineer or architect shall affix to the item his seal and the notation "altered by" followed by his signature, date of such alteration and specific description of such alteration.

WORK ZONE TRAFFIC CONTROL SIGN TABLE						
SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY		
W13-1P	A	24"X24"	30"X30"	30"X30"		
W14-3	A	36"X36"	36"X36"	36"X36"		
W13-4P	A	48"X48"X36"				
W16-1P	NOTE 3 A OR F	18"X24"	24"X30"			
W16-2P	A	24"X18"	30"X24"			
W16-4P	NOTE 3 A OR F	30"X24"				
W16-5PL W16-5PR	A	24"X18"				
W16-7PL W16-7PR	NOTE 3 A OR F	24"X12"	30"X18"			
W16-9P	NOTE 3 A OR F	24"X12"	30"X18"			
W20-1	A	36"X36"	48"X48"	48"X48"		
W20-2	A	36"X36"	48"X48"	48"X48"		
W20-3	A	36"X36"	48"X48"	48"X48"		
W20-4	A	36"X36"	48"X48"	48"X48"		
W20-5	A	36"X36"	48"X48"	48"X48"		
W20-5a	A	36"X36"	48"X48"	48"X48"		
W20-7	A	36"X36"	48"X48"	48"X48"		

	WORK ZONE	TRAFFIC (CONTROL SIGN T	ABLE	
SIGN	SIGN DESIGNATION	COLOR CODE	CONVENTIONAL ROAD	EXPRESSWAY	FREEWAY
	W21-1	A	36"X36"	48"X48"	48"X48"
SLOW MOVING VEHICLE	W21-4	A	36"X18"	48"X24"	48"X24"
SHOULDER WORK	W21-5	A	36"X36"	48"X48"	48"X48"
LEFT SHOULDER QLOSED RIGHT SHOULDER QLOSED	W21–5aL W21–5aR	A	36"X36"	48"X48"	48"X48"
LEFT SHOLLDER LOSED AHEAD LEFT SHOLLDER CLOSED XXX FT SHOLLDER CLOSED AHEAD RIGHT SHOLLDER CLOSED AHEAD RIGHT SHOLLDER CLOSED XXX FT SHOLLDER CLOSED XXX FT X MLE	W21-5bL W21-5bR	A	36"X36"	48"X48"	48"X48"
MOWING AHEAD	W21-8	A	36"X36"	48"X48"	48 " X48"
BLASTING ZONE AHE AD BLASTING ZONE 1500 FT 1/2 MLE	W22-1	A	36"X36"	48"X48"	48"X48"
TURN OFF 2-way radio and cell phone	W22-2	A	42"X36"	42"X36"	42"X36"
END BLASTING ZONE	W22-3	A	42"X36"	42"X36"	42"X36"
NEW TRAFFIC PATTERN AHEAD	W23-2	A	36"X36"	48"X48"	48"X48"
$\langle \rangle \langle \rangle$	W24-1L W24-1R	A	36"X36"	48"X48"	48"X48"
	W24—1aL W24—1aR	A	36"X36"	48"X48"	48"X48"
	W24-1bL W24-1bR	A	36"X36"	48"X48"	48"X48"

ROADWAY DEFINITIONS:
CONVENTIONAL ROAD – A STREET OR HIGHWAY OTHER THAN
A FREEWAY, OR EXPRESSWAY.
EXPRESSWAY – A DIVIDED HIGHWAY WITH PARTIAL CONTROL
OF ACCESS.
FREEWAY - A DIVIDED HIGHWAY WITH FULL CONTROL OF ACCESS.

COI	COLOR CODE LEGEND					
CODE	DESCRIPTION					
А	BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND					
В	BLACK LEGEND AND BORDER ON A WHITE BACKGROUND					
С	WHITE LEGEND AND BORDER ON A GREEN BACKGROUND					
D	WHITE LEGEND AND BORDER ON A RED BACKGROUND					
E	RED LEGEND AND BORDER ON A WHITE BACKGROUND					
F	BLACK LEGEND AND BORDER ON A FLOURESCENT YELLOW GREEN BACKGROUND					
G	WHITE LEGEND AND BORDER ON A BLUE AND RED BACKGROUND					

NOTES:

- DIMENSIONS ARE SHOWN AS WIDTH X HEIGHT.
 FOR SIGNAGE NOT SHOWN ON THESE TABLES REFER TO
- THE M.U.T.C.D. 3. WHEN USED IN CONJUNCTION WITH A BICYCLE SIGN (W11-1) OR PEDESTRIAN CROSSING (W11-2) COLOR CODE SHALL MATCH.

